



Dave Mackay page 38



Larry Faulkner page 78



Noel Widdifield page 70



# NYCENTRAL MODELER



**Join the New York  
Central System  
Historical Society  
(NYCSHS)**

**Today**

[www.NYCSHS.org](http://www.NYCSHS.org)

[www.NYCSHS.net](http://www.NYCSHS.net)



*Lars-Erik Sondenkamp shows us New Eastbrook* p. 32

*Dennis Regan & Bob Keeler return to St. Louis's Prototype Modelers Meet* p. 41

*Bob Shaw continues building his O-Scale layout* p. 49

*Brad Andonian tells us about Pacific Limited USRA boxcars* p. 56

*Seth Lakin offers Part 2 of a NYC PS-1 Boxcar* p. 63

# NYCSHS Modeler's E-zine

An added focus for the Society on NYC Modeling

## Lars-Erik Sondenkamp's New Eastbrook - A Factionous Town



A NYC layout in the Netherlands. Page 32

## Building a Big Four Freight House from a NYCSHS Offered Kit



Noel Widdifield builds an interesting kit offered in the NYCSHS Collinwood Shop. Page 70

Cover photos are of three NYC structures offered in the Collinwood Shop.

1<sup>st</sup> Quarter 2016  
Volume 6 Number 1

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## The NYCentral Modeler

The NYCentral Modeler focuses on providing information about modeling of the railroad in all scales. This issue features articles, photos, and reviews of NYC-related models and layouts. The objective for the publication is to help members improve their ability to model the New York Central and promote modeling interests. Contact us about doing an article for us. <mailto:NYCSHS@verizon.net>



## New York Central System Historical Society

The New York Central System Historical Society (NYCSHS) was organized in March 1970 by the combined efforts of several former employees of the New York Central Railroad. The NYCSHS is chartered as a non-profit corporation under the laws of the state of Ohio. The mission of the NYCSHS is to perpetuate the legacy of the [New York Central System](#) and its predecessor lines and subsidiary roads through the acquisition and preservation of their various histories, traditions, documents, records, and artifacts; and through the dissemination of accurate information in a manner that is consistent with good stewardship and preservation. Your membership gets you four issues of the popularly acclaimed

*Central Headlight*, the official publication of the [NYCSHS](#). The *Central Headlight* is only available to members, and each issue contains a wealth of information each quarter. From steam to diesel (and electric), from freight to passenger, from branchline to mainline, the *Central Headlight* covers it all. Our [Annual Meetings](#) focus on the preservation of New York Central [railroad history](#) with informative speakers, presentations, and tours. The Society also has many NYC reference [books](#) and [drawings](#) available for purchase. [Membership](#) is open to all; so don't delay; join today! [www.nycshs.net](http://www.nycshs.net)



### *Board of Directors*

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## The NYCSHS Modelers Committee

We have a NYCSHS Modelers Committee that now has 15 members. We meet on the phone about once a month to strategize about what to do to support NYC modeling for our members.

The members of the committee are Noel Widdifield, Ron Parisi, Brian Marotta, Dave Mackay, Ralph Schiring, Jeff English, Dave Staplin, Paul Pickard, Lawrence Faulkner, Manuel Duran-Duran, Kyle Coble, Dan Seligmann, Seth Lakin, Larry Grant, and Rich Stoving.

We have released over 50 models at up to 20% off MSRP to our members and have sold over 1,000 models and taken pre-orders for another 1,000. We have released several HO-scale models, one N-scale model, and are in the process of finding more HO-, N-, S-, and even O-scale models to be released in coming months. Watch our website and Collinwood Shop online store for more information. Website: [NYCSHS.org](http://NYCSHS.org) and Collinwood Shop: [NYCSHS.net](http://NYCSHS.net)

We have released 15 new NYC models over the last year, but if you have some ideas for us, contact us at [NYCBigFour@verizon.net](mailto:NYCBigFour@verizon.net)

# *A Chance Encounter*

*Rich Stoving, NYCSHS President*

Christmas is a time for traditions, and for Nancy and me one of the traditions of the season we have kept for many years is to listen to the wonderful recording that O. Winston Link made on Christmas Eve, 1957 of Norfolk & Western Train 42 making a brief station stop at Rural Retreat, Virginia. You've probably heard it, and if you haven't, you should. It co-stars N&W 603 and Mrs. J. E. Dodson on nearby church chimes. The last thirty seconds or so never fails to give me the willies.

Driving home with Nancy from a NYCSHS directors' meeting in Atlanta in September 1998, I glimpsed a sign that pointed the way to Rural Retreat, and we decided to visit the location where Link made his classic recording. We quickly found the small depot that is depicted on the record jacket.

Camera in hand, I decided to give NS a half hour to come up with a train. But nothing showed. Instead, a pickup truck arrived, and the driver came over to talk with us.

"Guess you all are railfans?" he drawled. I proceeded to tell him that his assumption was correct, and I went into a rather long-winded explanation about why this trackside location had a special meaning for us, including mention of the famous recording. The gentleman listened, nodding occasionally.

It turned out he was nodding very patiently, too, because this was his response: "I know. I was the fireman on the engine that night."

That seemed a bit too much of a coincidence, and I cloaked my skepticism as he introduced himself as Mr. James D. (Doug) Wohlford. But any doubts I had evaporated as he told us about his experience. "I was firing for Bill Brickey," he said. He went on to explain that the crew had instructions to stop at Rural Retreat, hold briefly, and then depart. They had no idea why, and really wondered what was going on when Link's multiple flash bulbs went off, filling the night with momentary brilliance. It was not until later that they understood what had happened, and Link sent Doug a print of his famous photograph, made simultaneously with the recording.



To make up for NS's failure to cooperate, Doug subsequently sent me the accompanying photo, taken at nearly the same angle as Link's photo, and we exchanged Christmas cards for many years.

How fortunate we were to enjoy this chance encounter, and to add to our many happy railfanning memories

*Rich*



## From the Cab

Each month I struggle with trying to determine the order to publish the articles we receive to publication. Usually, it is a real challenge to decide who gets first billing. That is because I find that all of our writers do a great job of sharing their modeling talents with us, so it is hard to choose who goes first.

One of the privileges of being the editor is being able to work with many of you to help get your story out. I love that part of the job.

One of the few frustrations is that very few of you ever take the time to let us know how we are doing. Usually, after the edition has been out for a couple of weeks, the writers send me an email or call me with the question, "What did people say about my article?"

We all are pretty proud of our models and modeling, but we like to hear what others think about our layout or skills. Once we take the time and effort to write an article, it is nice to get some feedback from the readers.

Please take a couple of minutes to tell us how we are doing. All it takes is a click on the link below and a few seconds to write a few words.

If you like what we do or have suggestions or comments take a minute to tell us. [NYCSHS@verizon.net](mailto:NYCSHS@verizon.net)

Also don't forget to send me your articles, comments, and/or corrections. [NYCBigFour@verizon.net](mailto:NYCBigFour@verizon.net)

Thanks, *Noel*  
Editor, *NYCentral Modeler*

## The *NYCentral Modeler*

This publication of the NYCSHS is for the purpose of providing NYCS modeling information. It is a publication by the NYCSHS Modeling Committee -- all rights reserved. It may be reproduced for personal use only. It is not for sale.

We encourage articles and photos submitted for publication. Materials submitted are considered to be gratis and no reimbursement will be made to the author(s) or the photographer(s) or his/her representative(s). The Society reserves the right to reject, for any reason, any material submitted.

We need articles for scales in addition to HO. We have published articles in Z-, S-, N-, HO-, O-, 1/29-, and 1/32-scales. We do need articles in all scales but want more non-HO articles.

Please contact the editor for submission information and guidelines. We have a style guide to be used for articles and will send it to any aspiring author. Photos should be submitted at not less than 640 x 480 pixels and in JPG, TIFF or PDF format. Statements and opinions made are those of the authors and don't necessarily represent those of the Society.

We make every effort to ensure all information is technically correct but do not guarantee it for accuracy. All articles and photos should be sent to: [NYCBigFour@verizon.net](mailto:NYCBigFour@verizon.net)

We really want to hear what you think concerning this issue and about your modeling or collecting. We will help you tell your story.

**Be sure to check out NYCSHS on Facebook!**

## NYCSHS Publications & Info

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NYCSHS Books: [NYCSHS Book Collection](#)

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NYC Passenger Cars: Dave Mackay: [mackdave@optimum.net](mailto:mackdave@optimum.net)

*Lars-Erik Sondenkamp* *New Eastbrook, A Town Along a NYC Mainline* Page 32



He was born in October 1956 in Sweden where his Dutch parents and two brothers lived during 1964 - 1970. In 1970 he moved back to The Netherlands. He now lives in a town near Haarlem (yes, the town of Haarlem which is the origin of the name of Harlem in New York!), which is some 20 kilometers from Amsterdam, where he currently works as a Human Resources Officer. At the age of eight he had his first model railroad car, a Märklin German Railroad reefer. Until the age of 20 he built a H0-scale Märklin layout with his two older brothers, Hans and Peter, at their parent's house. At the age of 20 he was distracted by some other interests like many at that age. In 2001, he, at the age of 36 with his two brothers and a friend, started an Ntrak modular club based upon North American prototype.

American Railroads had always been in his mind from the late seventies. The look and size of the American engines and trains impressed him. A Rivarossi catalogue with a beautiful drawing of a streamlined New York Central System Hudson leaving La Salle Street Station changed his interest forever. Since that time he has been a NYCS fan!

*Dave Mackay*

*Modeling a Small NYC Freight House*

Page 38



Dave spent his youth in Bergenfield, NJ, along New York Central's River Division, watching various freight trains of the "Road to the Future", such as WK-2, WD-5, and ML-12. This started a life-long quest for NYC River Division and NYC information.

He currently resides in Ringwood, NJ, with his wife Annette (a NYC China collector), and has amassed an HO Diesel collection currently at 80 different models, nearly every model the NYC had, although all of them aren't finished yet.

Dave's HO-layout is of the River Division in Bergenfield and Dumont during 1953. This also includes the NYO&W freight and passenger service.

His Society participation started when he attended his first NYCSHS Annual Convention in Albany, and wanting to help with growth of the Society. This was in part from the modeling assistance he has received from our President, Rich Stoving, in Dumont, NJ. He is currently the Director in charge of Membership, as well as a member of the NYCSHS Modeling Committee (and the Shipping Department).

Dave is the Training Director for a Hydronic Heating Manufacturer's Rep. Agency in the Metropolitan NY & NJ Area.





This is the fourth time that Dennis and Bob have represented the NYCSHS at the St. Louis meet. They continue to represent the NYCSHS at this meet each year by taking some of the books that the Society has on their website to interest visitors at the meet to try out the “Collinwood Shop”, NYCSHS’ online store. They also brought some of their models and a lot of enthusiasm to impress visitors at the show.

## *Bob Shaw*

### *Building a Simple O-Scale Lift-Out Bridge*

Page 49



A native of Schenectady, NY, Bob’s interest in the New York Central began when he was a boy and continues to be expressed today in the O-scale trains he runs, and the layouts he builds. He enjoys the great friendships that were forged as a member of TCA (Train Collectors Association) and LOTS (Lionel Operating Train Society).

Bob and his wife Wanda are retired and live in southern Michigan. When not “training”, they enjoy visiting their married twin sons, “spoiling” their two grandchildren, traveling, and getting together with friends. We are pleased that Bob is sharing his layout experience with us again in the forth of a series that takes us through the process of planning, designing, and building his creative interpretation of the famous NYC Water Level Route.

## *Brad Andonian*

### *Pacific Limited O-Scale Boxcars (USRA Variants)*

Page 56



Brad Andonian is the third generation owner of Pande Cameron Rugs; an importer and retailer of fine hand knotted carpets in Seattle. He began like many with Lionel in his youth; he re-entered the hobby five years ago and switched to 2-rail. An eBay listing for brass tank cars from Japan served as

a break from 3-rail and he went “all in”. Brad and pals are currently building a 30’ x 50’ double tracked loop, Lehigh and Western with an emphasis on late steam. Heavy industry is the key as he wishes to employ his NYC cement gondolas in an active format. He has a nearly complete collection of Pacific Limited brass NYC boxcars and has been painting, decaling, and weathering them. He seeks out brass models that either match or are close to NE road prototypes and often has custom decals made to suit the models. He wife and children has been very supportive of his hobby and his Thursday night with the trains. He hopes to continue to supply the Society with articles detailing accurate O-scale brass models of the NYC and its subsidiary lines.

*Seth Lakin*

*NYC's 50' PS-1 Boxcars - Part 2 Lot 136-B*

Page 63



Seth Lakin grew up a few blocks away from the crossing of NYC's Water Level Route and Michigan Central mainline at Porter, IN. However his earliest railroad memories are not of NYC trains but of diesels of the late 1970s and early 1980s Conrail. This is Seth's fifth article for us and he starts a two-part series on the 50' PS-1 boxcars. Seth is on the NYCSHS Model Committee and is a prolific modeler.

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*Noel Widdifield Building an American Model Builders' Big Four Freight House* Page 70



Noel has been the editor of the *NYCentral Modeler* since the first edition in 2011. He is a long-time modeler and fan of the NYCS. He grew up along the Big Four in Anderson, IN, and has modeled in S-, HO- and Large Scale since he was very young.

He flew airplanes in the USAF over a 20-year career. He worked in the aerospace industry for thirty years and retired recently after selling his own corporation.

He has been a NYCSHS member for many years and a Director since 2011. In addition to editing this magazine, he serves as the Business Manager, Chair of the Modelers, and Advertising Committees. He is the Webmaster for the NYCSHS website, Collinwood Shop, NYCSHS Facebook page, and the NYCSHS Members' Yahoo Group.

Noel has been married to Ann for over 50 years and has a son, David and daughter, Julie, (Proof Editor for the *NYCentral Modeler*) and has five wonderful and beautiful granddaughters. The oldest, Kristin (photo above), was just married.

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This article details his first attempt at HO-scale model building in almost 30 years.

## The NYCentral Modeler Needs Some Articles

We are able to continue to publish this magazine because a few of you have been willing to send us photos and articles about NYCS modeling. We know from our surveys that we have almost 1000 NYCS modelers out there in the membership. I know that many of you think that you don't have the ability to write an article, but we all do. We can provide you with tools to help write an interesting and informative article for us. We really want to know more about your collecting, modeling, or your layout. It only takes a couple of hours and a camera or cell phone to create a short article. All it takes is a phone call or email to us to get you started. Help us help you to tell us about your hobby. We are interested in all scales and all of the different ways we share this hobby. Please send us an email or give us a call so we can help you get started. [NYCSHS@verizon.net](mailto:NYCSHS@verizon.net) or 703-407-3059

Thanks, *Noel* Editor, *NYCentral Modeler*





# What's New for NYCS Modeling

By Noel Widdifield



## Z-Scale Locomotives

### American Z Lines



E-8 NYC

Road #s 4057 & 4058

MSRP \$176.00      Expected Release Now



### NYC (NYMX) 51'3" Mechanical Reefer

These 51' 3" mechanical reefers with rivet sides, single plug doors and bearing roofwalks, are painted aluminum with yellow sides with blue bands. They bear the 20" blue and white oval New York Central logos, white reporting marks/road numbers, 'Mechanical Reefer' designations, and black small lettering. They were built in July 1956 by Pacific Car & Foundry of Renton, WA, and run on Roller Bearing trucks.

Available weathered or new looking.

MSRP \$28.90      Expected Delivery Now

<http://www.americanzline.com/>

## N-Scale Rolling Stock

### Bluford Shops



### USRA 2-Bay Hoppers CCC&StL & NYC

More than 55,000 of these cars were built making them one of the most prolific freight car designs of the twentieth century. These ready-to-run cars will feature: diecast slope sheet-hopper bay-center sill assembly; injection molded plastic sides, ends, and hopper doors; fully molded brake tank, valve and air lines; body mounted brake hose detail; coal load; lever-style hand brake; body mounted magnetically operating knuckle couplers; close coupling; and Fox Valley Models metal wheels. All road names will be available in multiple road numbers.

MSRP \$23.95 each, 2-pack \$47.90 & 3-pack \$71.85

Expected Delivery 2<sup>nd</sup> Qtr 2016

Also available in Collinwood Shop

<http://www.bluford-shops.com/>

### Intermountain Railway Company



This N-scale 1937 AAR 40' Boxcar is painted oxide red with white lettering. The oval New York Central logo is white. The model is equipped with MicroTrains® trucks and couplers.

Built: NEW 7-42

### Stock Number / Car Number

-25 157067	-26 157118	-27 157325
-28 157583	-29 157649	-30 157790

MSRP \$22.95      Expected Delivery Mid-2016

<http://www.bluford-shops.com/>



# What's New for NYCS Modeling

By Noel Widdifield



## Wheels of Time



### NYC 53'6" Welded Fishbelly Flat

These General Service 53'-6" flat cars were constructed with a welded fish-belly frame. The fish-belly frame provided an optimal distribution of tension and compression forces from the center of the car to the bolsters. In addition, fabricating a welded frame was less labor intensive than an equivalent riveted frame. The welded frame proved extremely durable holding up to really rugged service where the wood deck or the nail able wood-steel deck would need to be replaced a couple of times during the life of most cars. These flats commonly found their way into maintenance-of-way service because they were still in good shape beyond the AAR interchange car-age limit. A number of builders constructed these flat cars such as the Pacific Car & Foundry (P.C. & F.), Marine Industries (Canada), Pullman-Standard, Bethlehem Steel, and including individual railroad car shops.

They were designated as FM by the AAR mechanical people. They carried anything bulky that couldn't be placed in boxcars from tractors to finished lumber.

### Wheels of Time Model Features -

- Properly weighed
- Proper ride height
- Body-mounted knuckle couplers
- 33" metal wheels
- Extra fine details
- Authentic Paint, lettering, car numbers
- Eight car numbers

**MSRP \$22.95 Eight Cars (All Car #s) \$183.60**

**Taking Pre-orders until December 30, 2015.**

**Expected Delivery 2nd Qtr. 2016**

<http://www.wheelsovertime.com/>

## N-Scale Vehicles

### Trainworx



### Flexi-Vans

40432-(01-03) New York Central dual mail doors

Flexi-Van Service

40432-(04-06) New York Central dual mail doors

US Mail

40432-(07-09) New York Central Flexi-Van service

40432-(10-12) New York Central Pittsburgh & Lake Erie

40432-(11-15) New York Central Cigar Band

**MSRP \$17.95 Expected Deliver 2<sup>nd</sup> Qtr. 2016**

<http://www.train-worx.com/>

## N-Scale Structures

### The N Scale Architect



## Lines East Signal Stations

For complete description see this listed under HO-Scale Structures below.

MRSP \$69.95 Members \$52.46

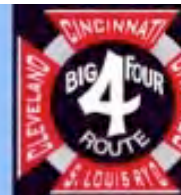
Expected delivery Jan 2016

**These are available in the Collinwood Shop.**



# What's New for NYCS Modeling

By Noel Widdifield



## HO-Scale Locomotives

### Bachmann



### USRA Light Mikado 2-8-2

The latest in Bachmann's USRA series of locomotives, the 2-8-2 is also known as the "Mikado" after Baldwin delivered the first engines with this wheel arrangement to Japan Railway in 1893. The DCC sound-equipped USRA Light model includes our Sound Value *SoundTraxx*® steam sound package with authentic prototypical chuff, short and long whistles, bell, air pump, steam release, and blower—all in 16-bit polyphonic sound. Performs best on 18" radius curves or greater.

NYC only had a very few of these USRA locos. They do not represent the vast majority of NYC Mikados.

MSRP \$399.00      Expected Delivery Now  
<http://shop.bachmanntrains.com/>

### Athearn



### EMD NYC Model 40

The **EMD Model 40** was a two-axle diesel-electric locomotive built by Electro-Motive Corporation, and its corporate successor, General Motors' Electro-Motive Division (EMD) between August 1940 and April 1943. Nicknamed "critters", eleven examples of this locomotive were built. Powered by twin General Motors Detroit Diesel 6-71 diesel

engines, which produce a combined 300 horsepower (224 kW). Original buyers included the Electro-Motive Corporation/EMD Plant #2 switcher, Defense Plant Corporation - 4 units, the United States Army - 3 units, and the United States Navy 2 - units, and General Motors Cleveland Diesel Division - 1 unit.

### FEATURES:

- Separately-applied horn, exhaust stacks, bell
- See-through cab windows
- Fully-assembled and ready-to-run out of the box
- Painted and printed for realistic decoration
- Highly detailed, injection molded body
- Celcon handrails for scale appearance
- Quick Plug™ plug and play technology: DCC ready with 8 pin connector
- Bi-directional constant lighting so headlight brightness remains consistent
- LED lighting
- McHenry operating scale knuckle couplers
- All-wheel drive with precision gears for smooth and quiet operation
- Heavy die-cast frame for greater traction and more pulling power
- All-wheel electrical pickup provides reliable current flow
- RTR motor with precision-machined flywheels for trouble free operation
- Wheels with RP25 contours operate on Code 55, 70, 75, 83, 80, 100 rail
- Window packaging for easy viewing, plus interior plastic blister safely holds the model for convenient storage
- Replacement parts available including motor brushes
- Minimum radius: 15"

MSRP \$99.98      Expected Delivery Sept. 2016

Neat little loco, 11 produced, but the NYC didn't own any.

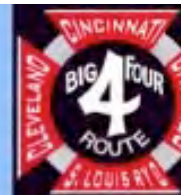
<http://www.athearn.com/>





# What's New for NYCS Modeling

By Noel Widdifield



## HO-Scale Rolling Stock

### Accurail



- 36' Double Sheathed Wood Boxcars
- 1300 Series NYC, MC, B&A & CCC&StL
- 1700 Series NYC&HR, LS&MS & CCC&StL
- 1800 Series T&OC

Accurail is announcing this car for release in late 2016. The New York Central and Hudson River after building their 1892-1902 standard boxcar design for ten years, redesigned their standard boxcar based on a circular issued December 2, 1901, by the Master Car Builders' Association and adopted by the American Railway Association which recommends standard outside and inside dimensions. The recommendations were made with Eastern Railroads in mind. The new NYC&HR car doesn't follow those recommendations to the letter but is close. The absence of a hood over the door was to help comply with width clearances. These new cars were labeled F-8 design and the cars were first produced in 1903. Later in 1907 a continuous metal draft sill was added to the design and the car was enlarged from 36' 10" over the sill to 37' over the sills. This design was also used by the the Big Four and Michigan Central.

We believe that the Accurail car is a model of the next generation/s of boxcars first built in 1910 and shouldn't be confused with the 1902/07 versions.

**MSRP TBA Expected Delivery Late 2016**

**These will be available in the Collinwood Shop.**



**NYC 50' Double Door Steel Boxcar**  
**MSRP \$16.98 Expected Release Early 2016**  
 These are available in the Collinwood Shop.  
<http://accurail.com/accurail/5200.htm>

### Intermountain Railway Company



**1937 AAR 40' Boxcar**  
**45701**

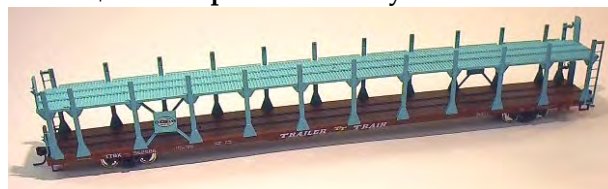
This HO- 1937 AAR 40' Boxcar is painted oxide red with white lettering. The oval New York Central logo is white. The model is equipped with metal wheelsets and Kadee® couplers.

**Built: NEW 7-42**

**Stock Number / Car Number**

- 13 157067 -14 157118 -15 157325
- 16 157583 -17 157649 -18 157790

**MSRP \$34.95 Expected Delivery Mid-2016**



**NYC 89-Foot Open Auto Racks (Bi-Level)**

The bi-level open auto rack consists of a TTBX TrailerTrain flat car fitted with a New York Central rack.

**MSRP \$21.98 Expected Delivery Now**

<http://accurail.com/accurail/>

### Moloco



**NYC GA 50' RBL Sill 1/ 10' 6" Offset Door/Narrow Rods Boxcar**



# What's New for NYCS Modeling

By Noel Widdifield



## Features

- Newly tooled Overhanging Stanray roof
- Perfect to prototype Stanray R3-4 welded ends, with GA style ladders and crossover platforms
- Car sides welded, with GA style ladders, fitted with specific-to-prototype door stops, door gussets and sill design
- Prototype correct handbrake specific to customer orders
- General American cushioned frame and floor configurations depending on prototype with Moloco's cushioned draft gear coupler boxes
- Appropriate cushioning devices, Hydra-Cushion or Keystone
- Appropriate to prototype trucks
- Rubber air hoses (MOLOCO)
- Four road numbers

- Correct 33" turned-metal wheelsets
- Proto MAX(TM) metal knuckle couplers

**MSRP \$24.98**

**Expected Delivery Now**

<http://www.walthers.com/>

## Rapido



**NYC NSC 73'6" Express Baggage Car**

### Features:

- Super-detailed underbody
- Body-mounted Micro-Trains® couplers
- All air, steam and electrical lines represented
- Insulated 36" metal wheelsets (no pizza cutters!)
- Diaphragms and safety bars
- Painted grab irons applied at the factory.
- Four numbers plus unnumbered available in each scheme
- Will operate smoothly on curves down to 9-3/4" radius
- Standard End or End Doors as appropriate
- Super-detailed Commonwealth Cast Pedestal tri-axle, 41-N-11 Inside Swinghanger or 41-BNO-11 Outside Swinghanger trucks as appropriate for each roadname and car types

**MSRP \$49.99**

**Expected Delivery Soon**

<http://www.molocotrains.com/>

## Walthers



**New York Central #167001 40' PS-1 Boxcar  
(Jade Green, System logo)**

Walthers Part # 910-2355

Road #s 167001, 167007

Limited edition - one time run of these road numbers

Thousands of cars built in this style from 1947 to 1949. Used in all types of general freight service into the 1980s. Based on early postwar production cars.

### Features:

- Correct car ends (no rectangles)
- Correct roof with flat end panels
- 6' Superior doors
- 2 different car numbers for each roadname

**MSRP \$54.95 Expected Delivery Spring 2016**  
**Also available in Collinwood Shop**

<http://rapidotrains.com/>





# What's New for NYCS Modeling

By Noel Widdifield



## Skytop Models



## NY C 60 ' Heavyweight Baggage Car

New York Central had several variations of 60-foot heavyweight baggage cars. They were acquired from several builders and hence were not all identical. There was an excellent two-part article in the March and April 2005 issues of *Mainline Modeler* that chronicled the various car bodies. The Skytop Models casting is closest to the type 4 carbody built by Osgood-Bradley in 1923. NYC subsidiaries such as B&A, CCC&StL, MC, NKP, and P&E also ordered similar cars.

The castings consist of a carbody with underbelly details and a separate cast roof. The modeler will need to add diaphragms, brake wheels, trucks/couplers and of course paint/decals. Wheels of Time Heavyweight 4 wheel passenger truck, item #999009 are not supplied but recommended.

MSRP \$35.00 Available Now

<http://www.skytopmodels.com/index.html>

## HO-Scale Decals

Resin Car Works



Resin Car Works has NYC decals available for older boxcars and newer hopper cars. There is also available an O scale set for USRA twins hoppers.

MSRP \$6.00 per set Available now

[http://www.resincarworks.com/decals\\_parts.htm](http://www.resincarworks.com/decals_parts.htm)

## HO-Scale Structures

The N Scale Architect

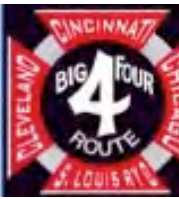






# What's New for NYCS Modeling

By Noel Widdifield



## Lines East Signal Stations

The New York Central called Interlocking Towers “Signal Stations” in their timetables and station lists. Some were labeled based on an abbreviation of their location, and others were chronologically numbered from earlier “Signal Stations” that they replaced.

After WWI, the New York Central instituted an improvement program in many areas of New York and New Jersey, which resulted in the construction of many new towers.

Among these projects was the construction of the Castleton Cut-Off in 1924. This connected the River Division, the Hudson Division, and the Boston & Albany to the Mohawk Division via the West Shore, by-passing Albany and its westbound grade. That project added SS 90 (Stuyvesant) on the Hudson Division at the branch to the Alfred Smith Bridge, SS SM (Smith) at the junction of the B&A branch and the Hudson Division Branch, and SS SK (Selkirk) where the River Division met the new line on its way to Selkirk yard. Continuing west after leaving Selkirk, the new towers were SS RJ, (Rotterdam Junction) connecting the B&M to the West Shore, SS 7 (Carman), connecting the Mohawk Division to the West Shore, and SS11 (Hoffmans) connecting westbound traffic to the Mohawk Division.

1927 saw the River Division get a fourth track from Weehawken tunnel to Dumont, NJ. That addition required adding four more towers for the interlockings at TU (Weehawken), WE (North Bergen), FY (Little Ferry), and DU (Dumont).

The addition of a fourth track on the Hudson Division in 1929 between Garrison and Beacon resulted in a new Signal Station at the New Haven interchange at SS 50 (Beacon).

Utica had new towers installed at both ends in this decade, resulting in SS 30 and SS 31, SS 25 (Rochester), and SS 18 (Palmyra) receiving new towers as well as SS 33 (Chili Jct.), SS 42 (Corfu), and, on the Erie Division, SS BV at Bayview.

Some of these towers were in the middle of nowhere. Lacking the utilities to power the

interlocking, they were manually or “Armstrong” operated. This required multiple rods, pipes, and links running out of the front of the concrete wall to the switch points, locks, and signals. The “city” SS towers were electric. Weehawken, North Bergen, Little Ferry, Utica, Rochester, and Bayview come to mind.

These towers were as “modern” as the NYC went, as the pre-WWI towers were all wood. This design had a concrete floor and knee walls, and was brick to the 2<sup>nd</sup> floor windowsills. They had central heat, with cast iron radiators on the operator level (toilet also), with a coal-fired boiler on the lower level. The coal filler (a small metal hatch or a pair of wooden doors) was in the trackside wall between the stairs and the first window.

Only a few of these remain, (SS 30 and SS BV), but none are any longer operational.

The Lines East Brick Tower offered by the NYCSHS represents these towers and is a unique offering by the Society.

**MSRP \$87.50 NYCSHS Members \$70.00 Expected Delivery Now**  
**Shipping extra and Ohio Residents must pay Ohio sales tax.**

[www.NYCSHS.net](http://www.NYCSHS.net)

## S-Scale Locomotives

### River Raisin Models



**Lima 2-8-4 Early B&A Berkshire**

River Raisin has negotiated with Boo Rim Precision to import several versions of this loco, including 6 numbers for Boston & Albany. The model will be factory painted brass with 63” scale drivers and a DCC wiring harness. Production is dependent on sufficient pre-orders.

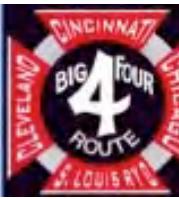
**MSRP \$2,299**

**Expected Delivery TBD**



# What's New for NYCS Modeling

By Noel Widdifield



<http://www.riverraisinmodels.com/>

## Des Plaines Hobbies



RS-1 Brass Body Kit

Des Plains Hobbies is offering this loco with all new details, lost wax castings and photo etchings with pre-formed hood. Complete illustrated instructions. They are designed to use American Models diecast trucks with brass side frames and Pittman motor all of which are available separately. **MSRP** \$335.00, Sideframes \$20.00, and Pitman motor \$40.00 when purchased with the kit. **Expected Delivery Now**

<http://www.desplaineshobbies.com/store/>

## O-Scale Locomotives

### MTH RailKing



NYC FM H10-44 Diesel Engine w/Proto-Sound 3.0  
Product # 30-20338-1 Road # 9105

#### Features:

- Detailed Durable ABS Body
- Die-Cast Truck Sides, Pilots and Fuel Tank
- Metal Chassis
- Metal Handrails and Horn
- (2) Handpainted Engineer Cab Figures
- Authentic Paint Scheme
- Metal Wheels, Axles and Gears
- (2) Remote Controlled Proto-Couplers
- Prototypical Rule 17 Lighting
- Directionally Controlled Constant voltage LED Headlights
- Lighted LED Cab Interior Light
- (2) Precision Flywheel-Equipped Motors
- Operating ProtoSmoke Diesel Exhaust

- Onboard DCC/DCS Decoder
- Locomotive Speed Control In Scale MPH Increments
- 1:48 Scale Proportions
- Proto-Sound 3.0 With The Digital Command System Featuring: Freight Yard Proto-Effects
- Unit Measures: 13" x 2 5/8" x 3 3/4"
- Operates On O-31 Curves

**MSRP \$329.95 Expected Delivery Jun 2016**

**Also available in Collinwood Shop**



NYC Pacific 4-6-2 w/Proto-Sound 3.0

Product # 30-1677-1 Road # 6508

#### Features:

- Intricately Detailed, Die-Cast Boiler and Chassis
- Intricately Detailed, Die-Cast Tender Body
- Authentic Paint Scheme
- Die-Cast Locomotive Trucks
- Metal Handrails, Whistle and Bell
- Metal Wheels and Axles
- Remote Controlled Proto-Coupler
- Prototypical Rule 17 Lighting
- Constant Voltage LED Headlight
- Powerful Precision Flywheel-Equipped Motor
- Synchronized Puffing ProtoSmoke System
- Locomotive Speed Control In Scale MPH Increments
- Wireless Drawbar
- Onboard DCC/DCS Decoder
- Proto-Sound 3.0 With The Digital Command System Featuring: Passenger Station Proto-Effects
- Unit Measures: 21" x 2 7/16" x 3 11/16"
- Operates On O-31 Curves

**MSRP \$449.95 Expected Delivery Feb 2016**



# What's New for NYCS Modeling

By Noel Widdifield



## NYC Empire State Express Hudson w/ ProtoSound 3.0

Product #s 20-3649-1 & 20-3648-1 HiRail Wheels  
Product #s 22-3649-2 & 22-3648-2 Scale Wheels  
Road #s 5429 & 5426

### Features:

- Detailed, Die-Cast Boiler and Chassis
- Detailed, Die-Cast Tender Body
- Authentic Paint Scheme
- Real Tender Coal Load
- Die-Cast Locomotive Trucks
- Handpainted Engineer and Fireman Figures
- Metal Handrails, Whistle and Bell
- Metal Wheels and Axles
- Remote Controlled Proto-Coupler
- O-Scale Kadee-Compatible Coupler Mounting

### Pads

- Prototypical Rule 17 Lighting
- Constant Voltage LED Headlight
- Operating LED Firebox Glow
- Illuminated LED Driver Lights

- Operating LED Marker Lights
- Lighted LED Cab Interior
- Operating Tender LED Back-up Light
- Powerful Precision Flywheel-Equipped Motor
- Synchronized Puffing ProtoSmoke System
- Locomotive Speed Control In Scale MPH

### Increments

- Wireless Drawbar
- 1:48-Scale Dimensions
- Onboard DCC/DCS Decoder
- Proto-Scale 3-2 3-Rail/2-Rail Conversion Capable

**MSRP \$1,199.95 Expected Delivery July 2016**



## NYC ALCo S-2 w/ ProtoSound 3.0

Product #s 20-20589-1 & 20-20590-1  
Road #s 9621 & 9631

### Features:

- Intricately Detailed Durable ABS Body
- Die-Cast Truck Sides, Pilots and Fuel Tank
- Metal Chassis
- Metal Handrails and Horn
- (2) Handpainted Engineer Cab Figures
- Authentic Paint Scheme
- Metal Wheels, Axles and Gears
- (2) Remote Controlled Proto-Couplers
- Prototypical Rule 17 Lighting
- Directionally Controlled Constant voltage LED

### Headlights

- Illuminated LED Number Boards
- (2) Precision Flywheel-Equipped Motors
- Operating ProtoSmoke Diesel Exhaust
- Onboard DCC/DCS Decoder
- Locomotive Speed Control In Scale MPH

### Increments

- Proto-Scale 3-2 3-Rail/2-Rail Conversion Capable
- 1:48-Scale Proportions
- Proto-Sound 3.0 With The Digital Command System Featuring: Freight Yard Proto-Effects
- Unit Measures: 12" x 2 3/4" x 3 3/4"
- Operates On O-31 Curves

**MSRP \$449.95 Expected Delivery May 2016**

<http://mthtrains.com/productline/RailKing>

## Bachmann-Williams



## NYC "Semi-Scale" 4-6-2 Pacific

### Features:

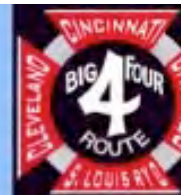
- *True Blast® Plus* 16 bit polyphonic sound system with whistle, bell, chuff, and idle steam sounds
- die-cast boiler and chassis
- powerful motor with flywheel coasting action
- traction tires





# What's New for NYCS Modeling

By Noel Widdifield



- operating headlight and back-up light on tender
- operating rear coupler
- operating smoke unit
- 3-Rail
- navigates O-31 curves
- length 19.95"; height 3.75"

**MSRP \$599.95 Expected Delivery 2016**

## O- Scale Rolling Stock

MTH



### NYC 50' Double Door Boxcar

Product # 20-93677 Road #s 62552 7 62550

#### Features:

- Intricately Detailed Durable ABS Body
- Metal Wheels and Axles
- Die-Cast 4-Wheel Trucks
- Operating Die-Cast Metal Couplers
- Separate Metal Handrails
- Fast-Angle Wheel Sets
- Sliding Car Doors
- 1:48-Scale Dimensions
- Unit Measures: 14 1/8" x 2 3/4" x 3 7/8"
- Operates On O-31 Curves

**MSRP \$69.95 Expected Delivery Mar 2016**



### NYC R50B Express Reefer

Product # 20-94270 Road #s 9392 & 9390

#### Features:

- Detailed Durable ABS Body
- Metal Wheels and Axles

- Die-Cast 4-Wheel Trucks
- Fast-Angle Wheel Sets
- 1:48-Scale Dimensions
- O-Scale Kadee Compatible Coupler Mounting Pads

• Unit Measures: 14 1/2" x 2 1/2" x 3 1/2"

• Operates On O-31 Curves

**MSRP \$74.95 Expected Delivery Apr 2016**



Product # 20-65275 Road #s Numerous

#### Features:

- Intricately Detailed, Durable ABS Bodies
- Stamped Metal Floors
- Detailed Car Undercarriage
- Die-Cast 4-Wheel Trucks
- Needle-Point Axles
- (2) Operating Die-Cast Metal Couplers
- O-Scale Kadee-Compatible Coupler Mounting Pads

• Constant Voltage Overhead LED Interior

#### Lighting

- End-of-Car Diaphragms
- Separate Metal Handrails
- Detailed Car Interiors
- Sliding Baggage Car Doors
- 5-Car Sets Feature (1) Baggage, (3) Sleepers, (1)

#### Observation

- 1:48-Scale Proportions
- Unit Measures: 95" x 2 7/16" x 3 1/2"
- Operates On O-42 Curves

**MSRP 5-car set \$469.95 Also available in smaller sets and some single cars. Expected delivery May 2016**

<http://mthtrains.com/>

## Magazines

### Model Railroad News Nov 2015



pp. 18 - 22.

**"Westerfield Models Stock Car"**  
by Tom Teeple

An excellent article about building one of the Westerfield resin model stock cars. Not a NYC



# What's New for NYCS Modeling

By Noel Widdifield



one, but the article does a good job of telling how Tom built, painted, and lettered the car. A good primer for building one of Westerfields NYC stock cars we have for sale in the Collinwood Shop.

## Model Railroader Oct 2015



page 20

### "Budget Priced Telephones for your Layout"

by Jim Hediger

NYCSHS member, Pete LaGuardia provides an interesting article about using analog corded telephones and some other inexpensive equipment to create a communication system for operating on a model railroad.

## Classic Toy Trains Dec 2015



### "RX for Stress"

by Roger Carp pp. 62 - 66

Photo by Ken Pellegrino of MTH O-gauge NYC Empire State Express and cars running on his 14' X 25' layout.

### "Letters From our Readers" page 6

Photo of NYCSHS member Gordon L. Peterson's detailed passenger car interiors. Gordon did an article for us and we have featured photos of his passenger cars in several issues.

## Classic Trains Fall 2015



### "Beauty and a Beast" page 9

A Ray Blanchard photo of NYC P-2 third-rail motor #238 piloting a Baldwin diesel-hydraulic on the *eXplorer* in mid-1956.

### "Just Before The End" pp. 38 - 45

by NYCSHS Member Fred B. Furminger

Although Fred took many photos of NYC steam locomotives, in this article Fred tells us about his

pursuit of steam loco photos long after the NYC ones were only history. A great story and some good photos, but no NYC ones.

### "Last Stop Before World War II" pp. 66 & 67

by Bill Graper

A story behind the photos found by the author of his grandfather engineer who piloted the NYC *Empire State Express* on its first run on Dec 6, 1941. A good story and some great photos.

## Classic Trains Winter 2015



### "Hudson Valley Hot Spot" pp. 20 - 29

by David W. Salter

An interesting article about David's quest to photograph some trains on his favorite railroad, the NYC. A great story and some wonderful photos of NYC steam and diesel locomotives pulling some passenger and freight trains along the four-track main North of Harmon.

## Classic Trains - More Trains of the 1950s



### "The Diesel's Big Year" pp. 14 - 23

by David P. Morgan

An article about 1950 and how American railroads invested a half a billion dollars in 4,027 new diesel locomotives. The article includes many great photos of the new diesels including one of a Baldwin NYC Shark on page 18.

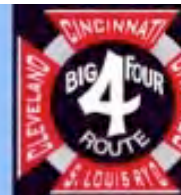
### "Kodachrome Decade" pp. 60 - 69

This issue has a beautiful series of color photos on the diesel locos purchased in the 1950s including a shot called Steam Stronghold on pages 60- 61 with a NYC J-1d Hudson in a line up at the Cincinnati Union Terminal roundhouse.



# What's New for NYCS Modeling

By Noel Widdifield



## "I'm a Railroad Fan" pp. 96 - 103 by David P. Morgan

The article is about Al Perlman and why he came east to run the NYC. A good story and some great NYC photos.

## Two New Online Magazines

### *The O-Scale Resource Online Magazine*

I received an email telling me about two online magazines that I had not been aware of before. They both provide some pretty interesting articles covering both O-scale and S-scale modeling. Check them out if you model in O- or S-!!!



<http://oscaleresource.com/>

### *The S-Scale Resource Online Magazine*



<http://sscaleresource.com/>

## NEWS

### Atlas Acquires Weaver Tooling



In a move that bolsters its already large back-catalog of O-scale model railroading products, Atlas has announced an agreement with Weaver

Models that brings a host of popular locomotive and freight cars into the fold.

"It was with sadness that we heard Weaver Models was shutting down operations," said Paul Graf, CEO of Atlas Model Railroad Company, Inc. "However, we are pleased to announce that we will be bringing many of their models back to the market so that they can be enjoyed by a new generation of model railroaders. These products will fit right in with our existing lines and we are committed to bringing them to market with the high quality that you've come to expect from Atlas."

Tooling acquired includes the 2-8-0, U25B, RS11 and VO-1000 locomotives, the Troop Sleeper and Kitchen Cars, the Pullman Bradley Coaches, the War Emergency Gondola, the Wagontop Box Car; and the H30 Covered Hopper. Molds for the 20' Containers and telephone poles have also been obtained.

## Mullet River Model Works



Glenn Guerra, owner of Mullet River Model Works, announced that he is closing the company. He has suffered a substantial enough loss due to the loss of a sub-contractor to not be able to recover from it. Also, being 65, he announced that it was time to retire. However, Glenn is the co-owner of *The S Scale Resource* and *The O Scale Resource*, which he indicated that he very much enjoys, so he is not going away. Although Mullet River was primarily an O-scale manufacturer, they did produce a number of S-scale cabooses. There are some still in inventory, but the entire S-scale (and HO-scale) product line has been sold to Des Plaines Hobbies (which apparently still has some in stock as well).





## NYCSHS RPO Letters & Emails



Hello Noel,

I finally got around to opening up some model kits I purchased at the Utica convention back in May, and discovered that the hopper car inside the T&OC box is lettered for the Cincinnati Northern. While I have nothing against Cincinnati, I'm from Toledo, so it was the T&OC car that I really wanted. Should have checked inside the box before leaving the store table.

Could I mail this car back to you and exchange it for a T&OC car? I suppose we can also swap at the next Convention in Chicago, too, if you believe you will still have the T&OC cars in stock next spring.

Thanks for all you do for the Society; all the models that have come out recently look fantastic!

**Dean Pyers**  
**Sterling Heights, MI**

**Dean,**

Sorry about the mix-up with the hopper. Mail us the old one and we will send you the correct one.

Thank you for the kind words about our models. We are trying to provide a wide range of NYCS models in all scales.

**Noel**

I was wondering if I could get an update on my order number: NYCSHS-2816. The delivery date was supposed to be the 2nd quarter of 2015. I have not yet

received this order, so I was wondering if an updated shipping date is available?

Thank you,  
**Sal Sgroi**  
**Frankfort, NY**

**Sal,**

The Atlas shipping schedule on their website still says 2nd Qtr. 2015.  
<http://www.atlasrr.com/ordership.htm>

That is the best information we have on the models. They don't always keep us up to date on these things, but I believe they should be shipping soon. I have a pair on order myself.

Thanks for your order and your patience. We have no control over the manufacturers or their shipping schedule. As soon as we have better info we will post it and will ship your RS-3 as soon as we get it from Atlas.

**Noel**

I appreciate the update. One reason why I inquired was that if I read the info in my order history correctly, the indication was that the order had shipped. Since I hadn't received it I was concerned maybe it was lost in shipping. I am relieved to find it is only a shipping delay.

Thanks for taking the time to respond. I've been a member of NYCSHS since February and I am extremely impressed with all the group does and offers.

**Sal**



**Sal,**

One of the interesting things about our online store is that when we approve a purchase it sends the email saying it is shipped. It is a problem with the software that we have been unable to get them to fix.

Thanks for being a member, purchasing our products and for you kind words.

Thanks, **Noel**

Hello **Noel,**

I am so glad that you have issued this DVD with all the *Central Headlight* issues because I have read with greatest interest the report of Thomas R. Gerbracht on the famous Niagara locomotive in the 3rd issue of 1988, but I tried in vain to find a copy of the 1st issue of 1989 with the second and final part of this exciting paper.

Best regards,

**Wolfgang Herfs**  
**Germany**

**Wolfgang,**

I hope you enjoy the DVD. There is a tremendous amount of information about the NYCS in it and it is searchable.

Thank you for your order,  
**Noel**

Dear **Noel,**

Please be informed that your DVD just arrived here today! It took me some 16.25 \$ German import tax (which I find an incredible high amount as compared to the price of the DVD), but after a first look into the content of the DVD I am so positively excited and extremely glad that I bought it!!!

The New York Central System Historical Society has done a great job in compiling this DVD, which is a real treasure for us, railway and steam enthusiasts.

I became aware of the NYC Niagara locomotive through a paper of the well known Arnold Haas in the German Lok Magazine no. 8 (1964), and many years later I happened to find in the web the *Central Headlight* issue no. 18. 3 (1988) with the first part of the very interesting and detailed paper of Thomas R. Gerbracht on this famous locomotive. Since then I tried during quite a number of years without success to get hold of the second part of this paper.

And now I got it together with a huge wealth of other NYC related Information contained in your DVD.  
I am so happy!

I send you my best regards from the northwestern corner of Germany (not far from the Dutch border),

**Wolfgang**

**Noel,**

I am most happy you are offering the NYC brick Signal Station for us modelers. You



see, it is accurate for a few such installations, one being SS25 in Rochester. It so happens that is the tower I spent most of two summers visiting almost daily in 1955 and 1956! A kind operator named Ed Cormier took me under his wing whenever he was working. It was fascinating for me as an eleven year old interested in only railroads. I was just drawn to it.

**Warren Frost**  
**Vacaville, CA**

**Warren,**

I am sure the model will add to your wonderful memories. Hope you enjoy it.

Thanks, **Noel**

Hi **Noel,**

I apologize if I'm a pest but the "Limited Edition" of model railroad marketing these days keeps me on the edge of my seat; particularly when I see evidence that there may be a SNAFU in progress which could prevent my receiving specific models which I have, in good faith, preordered as instructed in order to assure receipt of them when available. Unfortunately, for you, you are a "Human" to whom I have access and to whom I can address questions of availability when early signs of a mix-up appear and it is not yet too late to be fixed. I empathize with the poor folks who aren't lucky enough to have such a contact in this world in which many orders are a "crap-shoot" that can end up in the "big computer in the sky", never to be heard from again.

Anyhow, on 1/28/15 I placed order 2785 with the Society for the new ESM well car (1) and for a 3 pack of the new Atlas P&E 2-bay hoppers. I received the well car mid summer, paid the remaining sum, and all was fine but the order should still be open since I have not heard anything about the hoppers yet for which I paid \$45.48 which I believe to be the full price (not a deposit). Now I see by this latest mailing that we should "order soon because there are very few remaining" and, according to the Society website, they are scheduled to be shipped 3Q2015 which is already history.

Something is obviously goofy when the Society is selling off the "few remaining" when I have not yet seen my preordered 3 pk. FYI (just to avoid possible confusion) I also have outstanding orders for the Bluford B&A 3 bay hoppers, P&LE 3 bay hoppers, and CCC&StL 2 bay hoppers, as well as various boxcars from Atlas and BLI.

Please check that I am still in line to receive the P&E 2 bay hoppers. These are of special concern because, as I'm sure you know, there has always been a dearth of N-Scale models of the P&E and I was excited when these were announced.

Again, I'm sorry to bother you with this but it seemed the best way to get a satisfactory solution to my concern.

Thanks,  
**Chuck Baldwin**  
**Penfield, NY**





**Charles,**

1. We keep very accurate records of what has been ordered and track those orders until they are filled. Orders do not end up as a crapshoot and lost in the big computer in the sky. We haven't lost an order in the entire time we have been in business.

2. As to your Atlas P&E hoppers, Atlas keeps slipping the delivery schedule on these cars. Their website calls for a delivery in the 3rd Qtr. 2015 which does not end until Oct 1. Your cars will be shipped when we get them. WE HAVE NO CONTROL OF MANUFACTURER'S SHIPPING SCHEDULE. As to "only a few remaining" we must pre-order from Atlas a set number of cars and we still have a few of those left for sale. Hence the "order soon because there are very few remaining". Your cars are still on order.

3. As to your remaining orders. We have not lost them and will send them to you as soon as we get them from the manufacturer. They are not lost and will not be lost.

4. We have no control over the manufacturers. They tell us they will be making a model and we must provide them with an order for a number of the models before their order cutoff date. We then advertise the cars and tell you when they say they plan to ship them. The orders close and then they do what they do and ship them when they are ready. WE HAVE NO CONTROL OF MANUFACTURER'S SHIPPING SCHEDULE.

I wish that we did control the schedules but we don't. Sometimes they keep the schedules up to date and sometimes they

don't. WE HAVE NO CONTROL OF MANUFACTURER'S SHIPPING SCHEDULE.

I am afraid that this is how it works with the models. If you cannot put up with all of this, you might want to cancel your orders with us and go to a local dealer, who will have the same issues that we have.

We have been doing this for over two years and have not lost anyone's order yet. I don't know what else to tell you.

We work very hard to provide members with a wide variety of models at discounted costs. We are all volunteers and we do a pretty good job in a difficult sales environment over which we have little control.

I hope this resolves some of your concerns, but all of this will not change. It is the way the model business is today.

**Noel**

I am including this exchange because I get an email almost every day asking about the shipping schedule for one of the models. As I state in the note to Chuck, we simply have no control over the manufacturers. We are completely at their mercy as far as schedules. If you have concerns about your model, go to the NYCSHS website and look at the "Model Shipping Schedule" found under the "Modeling Resources" button on the main page. <http://nycshs.org/model-shipping-schedule/>



**Hello,**  
Has this order shipped? If so do you have a tracking number? If not when will it be shipped?

Order was taken August 28th and PayPal payment was taken August 31st. Today is September 16th.

**Mark Keigan.**  
**Bowmansville, Ontario, Canada**

**Mark,**

We received the tower kits on the 4th, and shipped your two kits on the 9th (had to get larger boxes, which we didn't have). I've attached the tracking info I just downloaded; they cleared customs on the 15th.

I would think you might have them by now.

Thanks, **Noel**

Hi **Noel,**

Thank you for your response. I haven't received them yet. Probably tomorrow.

I think I requested these models in the last survey. And then they came available through the NYCSHS store. That is awesome.

The money I have saved from my member discount has pretty well equaled a year's membership.

I plan on making some more purchases

from the "store" soon.

I feel the society has greatly improved since I have joined 20 years ago.

Have a great day and thanks again,

**Mark**

**Noel,**

The HO-gauge Bachmann passenger cars on Page 12 look similar to a set of Bachmann cars that are lettered for the NYNH&H. On the combine, the baggage car doors are too close to the passenger end than shown in pictures I have of NYC Combines.

**John Lauterbach**  
**Macon, GA**

**John,**

You are probably correct. We don't review all of the products. It is impossible for us to do that so we try to make comments about the ones we can. We do try to list everything any manufacturer produces in NYC livery and try to comment where we can.

Thanks for your observations,  
**Noel**

**Noel,**

One thing I'm not sure I saw in the article ("Old Dog New Trick= Lord of the Resin, by Phil Darkins, 4<sup>th</sup> Qtr. 2015) is that he was building a very old Westerfield resin



## NYCSHS RPO Letters & Emails



kit made of the old metal-filled dark grey (epoxy-type I believe) resin. That stuff is utterly diabolical to work with and has not been used by Westerfield in years.

All Westerfield kits are of a grey urethane that's not absurdly brittle and filled with drill-bit-dulling and snapping metal. It's an important distinction to make, as I would never tell anybody to build a kit made of that dark grey stuff as a first kit. It ruined building resin kits for a lot of people back then. It was like trying to work with glass. It's evil stuff...take it from a veteran of the resin wars :)

Thanks and best regards,

**Craig Zeni**  
Cary NC

**Craig,**

Thanks for the info. Glad they have changed the material.

Thanks, **Noel**

*Email sent to everyone who had pre-ordered the Intermountain MCR Composite Drop Bottom Gondola*

Good Morning,

Last year you pre-ordered an Intermountain MCR Composite Drop Bottom Gondola from the NYCSHS. After waiting almost 18 months for Intermountain to produce the car and with no clear production date in sight, we are refunding your money for the car.

This is the second car that Intermountain promised to produce and then did not

produce because they apparently did not get enough orders.

We apologize for this and wish that they had produced the car. It is still listed on the Intermountain website, but the production date keeps being pushed out and when we call them they tell us to look at the website. We don't believe they will produce the car.

Your money is being refunded through PayPal, credit card or a check depending on how you paid for it.

You should see the refund early this week if you haven't gotten notification of it already.

Thanks, **Noel**  
Noel Widdifield  
Chair, NYCSHS Modelers Committee  
NYCSHS Director

Noel,

While this is disappointing, I appreciate the NYCSHS taking a firm stand with this manufacturer. I believe this action will enhance our reputation as an organization that is serious about producing quality NYC equipment! Thank you!

Blessings  
**Doug Chapman**  
Montclair, VA

*This illustrates one of the problems we have with offering models for sale based upon manufacturers announcements and projected deliveries. They don't always deliver.*





**Hi Modeling Team:**

I find it very informative the historical descriptions that are provided with each of the models. I'm not that much of an expert on what equipment was used when, and having the background helps me select models that fit the time period of my layout.

It's very impressive how much is offered to members.

Keep up the good work.

**Ed Enyedy**  
East Lake, OH

Ed,  
Thanks for the kind words. Just keep buying from us. Your purchases of these models helps us keep membership costs down. We are trying to offer exciting and accurate NYCS models at competitive prices.

Thanks for buying and for being a member.

**Noel**

**Mystery Photo in 4<sup>th</sup> Qtr2015**  
*NYCentral Modeler Page 59*



*A shot of NYC Hudson making good time with lots of smoke showing. Can anyone tell us where this photo was taken?*

[NYCSHS@verizon.net](mailto:NYCSHS@verizon.net)

**Noel,**

It was taken at MP 35.6 Hudson Div., Croton on Hudson NY. Note the signal bridge, which in most photos included the whole bridge with signals, and yard lead in foreground.

Thanks,

**Victor Hand**  
Bar Harbor, ME

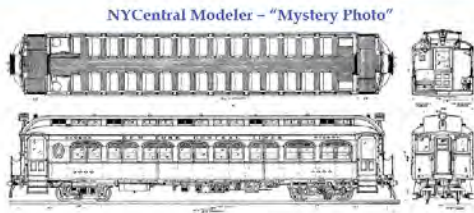
**Victor,**

You are correct. Thanks for your response.

**Noel**



**Mystery Photo in 4<sup>th</sup> Qtr2015 NYCentral Modeler Page 118**



Tell us what piece of equipment is in this drawing. Send your answers to [NYCSHS@verizon.net](mailto:NYCSHS@verizon.net)

**Noel,**

This was easy. # 3000 was the first in a series Electric MU used on the Hudson Division of NYC&HRRR just after electrification, which was around 1905. Here is a post card of the same. [IF YOU HAD PHOTOSHOPPED THE ROAD NO. OFF, THEN YOU WOULD HAVE AN EXTREMELY DIFFICULT mystery photo.]

**Wayne Koch**  
Mount Kisco, NY



**Wayne,**

You have the correct answer. Sometimes we do easy ones.

Thanks, Noel

**Noel,**

Use any way you wish. The photo is by me. Done in black and white to look more authentic.



*A Saturday afternoon local heads down the Hudson Division near Cold Spring, New York behind K-3q Pacific 4683 in the summer of 1949.*

**Rich Stoving**  
NYCSHS President

**Noel,**

Here is a photo of one of my finished cabooses.

Thanks,

**Erik**  
SPR Backshops



*Erik of SPR Backshops provides model construction of resin kits offered by WrightTRAK and Westerfields. Those kits and Erik's services are offered in the Collinwood Shops. See an additional photo of Erik's work on page 55.*



# New York Central System Historical Society, Inc.

## 2016 Convention & Meeting

Itasca, Illinois - May 13 – May 15, 2016

The Westin Chicago Northwest Hotel

400 Park Blvd., Itasca, Illinois 60143

### Schedule

#### Friday, May 13, 2016

- Registration
- Speaker Presentations
- Self-Guided Activities
- Collinwood Shop open

#### Saturday, May 14, 2016

- Bus to Union, Illinois, Illinois Train Museum [www.irm.org](http://www.irm.org)
- Convention Banquet at Westin Hotel
- Guest Speaker Mr. Nick Fry, Curator John W. Barriger III National Railroad Library
- General Members Meeting
- Collinwood Shop open following Meeting

#### Sunday, May 15, 2016

- Bus to the Museum of Science & Industry

Bus route will pass many long gone NYCS sites and drive by the Blue Island & Gibson yards

NYCSHS Website: [www.NYCSHS.org](http://www.NYCSHS.org)

Register in our store: [www.NYCSHS.net](http://www.NYCSHS.net)

Disclaimer: The schedule & list of speakers and layouts is the best that the committee understands as of Oct 13, 2015, and may change due to factors beyond its control.

### Historic Sites

- Chicago History Museum (Pioneer, Ex-MC Loco) [www.chicagohs.org](http://www.chicagohs.org)
- Chicago Union Station <http://www.chicagounionstation.com/>
- Chicago Dearborn Station
- Northwestern Oglivie Station
- Willis (Sears) Tower
- The Chicago Board of Trade
- Pullman Historic District <http://www.pullman-museum.org/>
- Chicago Art Institute
- Buckingham Fountain
- Chicago Museum Campus (Field Museum, Planetarium, & Aquarium)
- Architectural Boat Tours
- Chicago Pizza & Hot Dogs

### Hobby Stores & Layouts

- Lombard Hobbies, Lombard, IL
- Des Plaines Hobbies, Des Plaines, IL
- Golden Spike Train Shop, Burbank, IL
- Zientek's Hobby Shop, Chicago, IL
- Elmhurst Model RR Club (Friday night)

### Hotel Rooms

\$89 Plus Tax –Convention Rate. Deadline- April 21, 2016 for Special Rate. Reserve at Westin Chicago Northwest Hotel 1-888-627-8510 <http://www.westinchicagonorthwest.com>

### Presenters

- Ralph Eisenbrandt – “Michigan Central Joliet Branch”
- Darwin Simonaitis - “A Day on the Toledo West” “A Morning at Englewood 1966”
- Jim Suhs - “The Kankakee Belt”
- Alex Schneider - “Valley Cars – Pullman Plan 3988”
- Shel Lustig’s - “Experiences NYC Lines West”
- Carl Liba – “Chicago’s Other Central Station”
- Harold Edmonson – John Swajkart – “NYC End of Steam”
- Rich Stoving – “NYC’s 4-6-2 Pacifics”
- Tom Gerbracht – “General Electric and the NYC”
- Spencer Ziegler – “IHB Diesels”
- Tom Mitoraj – “La Salle Street Station
- David Staplin – “Converting a Walthers 4-4-2 Passenger Car”
- Frank Bongiovanni - “Laser Kits for Klutzes”
- Several Presenters - Modeling the NYC

Openings are still available. If you have a presentation you would like to give, contact Darwin Simonaitis at [dfssas@msn.com](mailto:dfssas@msn.com) or Jim Suhs at [jamesuhs@sbcglobal.net](mailto:jamesuhs@sbcglobal.net)

### Model Display Room

Currently includes a model of LaSalle Street Station and Mike Hill’s O-gauge Hudson collection. Bring your models for display.



# New York Central System Historical Society

## 2016 Convention & Meeting

Itasca, Illinois May 13 - May 15, 2015

Registration Form



### Please Print

Name of Attendee \_\_\_\_\_ Member # \_\_\_\_\_

Name(s) of Additional Attendee(s) \_\_\_\_\_

Address \_\_\_\_\_

City \_\_\_\_\_ State/Province \_\_\_\_\_ Zip/Postal Code \_\_\_\_\_

Country \_\_\_\_\_ Telephone \_\_\_\_\_ Email \_\_\_\_\_

Emergency Contact Name (Other than traveling companions) \_\_\_\_\_

Emergency Contact Phone Number \_\_\_\_\_ Relationship \_\_\_\_\_

Number of Attendees \_\_\_\_\_ Total Amount Paid \$ \_\_\_\_\_

Banquet Choices: Number of each selection. (Must add up to the number of registrations.)

Sliced Beef Bordelaise \_\_\_\_\_ Chicken Bruschetta \_\_\_\_\_ Vegetarian Meal \_\_\_\_\_

Please send payment & registration to: **NYCSHS**

**P.O. Box 67**

**Wood Dale, IL 60191**

[NYCSHS2016@att.net](mailto:NYCSHS2016@att.net)

**Please make checks payable to NYCSHS, Inc.**

### **Registration Fee:**

NYCSHS Members (Including family members) cost is \$179.00 each for all activities including the Saturday evening banquet. Non-members can register at \$179.00 plus a fee of \$39.00. The extra fee covers membership in the Society for one year, including all rights and benefits, along with one year of the *Central Headlight*.

Hotel Rooms have been made available at the: **Westin Chicago Northwest Hotel**

**400 Park Blvd**

**Itasca, IL 60143**

Please call the hotel directly at 1-888-627-8510. Rooms can be reserved at the Special NYCSHS rate of \$89.00 plus 11% tax. You must mention the NYCSHS Convention to receive the special rate. Early reservations are essential and are only available until April 21, 2016. The special rate is available for 3 days prior and 3 days after the Convention. Reservations will be filled on a first-come-first-served basis. Don't delay. **You are responsible for making your own reservations. They are not included in this registration.**

# NYCSHS Announces a New Exciting Book

## Know Thy Hudsons

by  
Thomas R. Gerbracht



## Know Thy Hudsons

- Detailed history of NYC Hudsons from official record cards & drawings.
- Covers all Hudson classes, Dreyfuss and Empire State streamlined locos.
- Over 250 pages and 200 photographs many previously unpublished, 32 in color.
- Written by NYCSHS Director and former president.
- Hard cover with complete Hudson history.
- MSRP: \$79.95 - NYCSHS Members: \$63.96 both plus \$5.50 shipping for US buyers. Non-US buyers contact us for shipping cost. Ohio residents must pay 8% Ohio sales tax.
- All proceeds go to the NYCSHS.
- Limited quantity of numbered & signed copies available at \$250.
- Expected 1st QTR. 2016 - Order Now!!!
- Online Ordering at [www.NYCSHS.net](http://www.NYCSHS.net) or download order form at:
- [www.NYCSHS.org](http://www.NYCSHS.org)

### Join NYCSHS

Only \$39.00 per year.

Great for fans, historians & modelers.

Many benefits. Join Today at:

[www.NYCSHS.org](http://www.NYCSHS.org)

NEW YORK CENTRAL SYSTEM HISTORICAL SOCIETY

P.O. BOX 130

GATES MILLS, OH 44040-130



## *New Eastbrook, A Fictive Town Along a NYC Mainline*

*Article by Lars-Erik Sondenkamp*

Why Ntrak and North American prototype for a NYC fan living in the Netherlands?

The interest for American Railroads has been in my mind as of the late seventies. One of my brothers, Hans, had some books about American Railroads and I was impressed by the look and size of the engines and trains. One day we bought the Rivarossi catalogue and on the front of the catalogue was the beautiful drawing of a streamlined New York Central System Hudson leaving La Salle Street Station. Ever since I'm a NYCS fan!

Due to the lack of enough space for a layout at home and to have the opportunity to run long trains, my two brothers, a friend, and I decided to start the Ntrak club PH&LF Railroad Association ([www.phlfrfa.com](http://www.phlfrfa.com)). Together we could build modular sections of a layout, which we could bring together at shows and run some long trains. Soon more members followed thanks to our website and the layout grew. In November 2003 we had our first appearance at a big Model Railroad show in the Netherlands and it was a great success. We ran long trains with a couple of engines in front. Our record train was 110 cars long, awesome!

From the start of building the modules, scenery and especially buildings had my interest. I want to create modules with scratchbuilt buildings to give it a different look than scenery with the same kits you see on other layouts. With the Internet I have a lot of possibilities to find interesting buildings for my modules. One

*Photos by Lars-Erik*

of my sources is the Library of Congress site: <http://www.loc.gov/pictures/>

But I Google a lot too for pictures of buildings. Furthermore one of my brothers has a lot of railroad books of the NYCS, although his main interest is the PRR (everyone makes some mistakes in his life ;-)). One of the buildings that has always caught my eye is the Great Lakes Terminal Warehouse in Toledo, OH, with the big sign on the site of the warehouse with the white letters on a black background.

The Terminal Warehouse is situated at the first of three Ntrak modules. On this module is a junction where a mainline of the PRR joins the tracks of the NYC mainline. The PRR has trackage rights for some miles to run on the NYC line. Given the busy junction, a signal tower at this point is highly desirable.



*GP30 #6118 and GP35 #6131 cross New Eastbrook junction in a late evening sun 1965. In the background New Eastbrook Terminal Warehouse and New Eastbrook tower.*

As I wanted to scratchbuild an original New York Central tower, I did some research and selected the now demolished West Detroit Tower. The real West Detroit Tower was located three miles from the huge (former) Michigan Central Depot at the mainline from New York via South



## *New Eastbrook, A Fictive Town Along a NYC Mainline*

Ontario, Canada, to Chicago. West Detroit tower was located at the intersection of this line with the Norfolk & Western/Detroit & Toledo Shore Line.

### **New Eastbrook Terminal Warehouse (Great Lakes Terminal Warehouse)**

For the size of the Warehouse I used Google *Streetview*. The building still exists and I took some “photos” of the building in *Streetview*.

<https://www.google.com/maps/streetview/>

Cars and people near the building were a reference for measuring the dimensions of the building. Although I’m not sure if the dimensions are correct the main purpose to build the Warehouse was to get the look and feeling of the original Warehouse. As a modeler you have to do what you like in your scale world, so I added a track for loading and unloading cars underneath the building, a track that doesn’t exist in the real world. But this gives me some nice switching possibilities.



*New Eastbrook Terminal Warehouse was still a busy place in the sixties. The building is of the same design as the Great Lakes Terminal Warehouse in Toledo.*

The building is made of styrene sheets. To get the concrete look I mixed a couple of paints and afterwards I weathered the

building. The windows are all made from clear sheets. I scribed the sheets with a sharp knife to get the windowpanes. The scribed lines I filled with a cloth with some paint in a way that the paint will fill the scribed lines and the windows themselves will stay clear.

For the lettering of the building I used the “tissue decal method”. I print the text or advertisement on one layer of tissue paper, which, I carefully stick to the building with diluted white glue.

### **New Eastbrook tower (West Detroit Tower)**

On the Internet I started looking for pictures of the tower. I had found a few photos of all sides of the tower. Based upon these photos and Google Sketchup (<http://www.sketchup.com/?gclid=CPuyg8nqj8kCFUUdgQodMD0Eog>), I could determine the (approximate) size of the building and could convert the building to N-scale.



*Another example of Lars-Erik’s scratchbuilding skills is this New Eastbrook tower (West Detroit tower) that he built from photos and the use of Google Sketchup.*

I made the walls from 1 mm thick styrene. These styrene walls were covered with Slaters brick sheets. The windows are made in the same way as I used for the warehouse.

## *New Eastbrook, A Fictive Town Along a NYC Mainline*

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To simulate tar roofing I also issued one layer of a tissue, which I cut in small strips. The strips were glued to the roof with diluted white glue to which I added some black acrylic paint.

Although the West Detroit doesn't exist in the real world anymore, it lives on in my modules.

### **Suggestions for a new building from NYCSHS members?**

The modules still need some buildings. At our last show - the NMRA UK Convention

in Derby, England, I used some temporary buildings from some other modules to fill a gap on the New Eastbrook modules. I'm looking for some industrial buildings, which were near a New York Central line. If anyone has some suggestions this would be very welcome, especially if accompanied with pictures and/or drawings.



## **Railfanning in New Eastbrook**

On a sunny October day I went railfanning in New Eastbrook and took some pictures.



*NYC FT #1601 with a freight train from Jasperburg passes New Eastbrook Tower.*



*New Eastbrook, A Fictive Town Along a NYC Mainline*

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*The tired looking NYC U25B #2509 is at the head end of a yard run from Jasper Yard.*



## *New Eastbrook, A Fictive Town Along a NYC Mainline*

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*NYC NW2 #8769 is switching some freight cars at the New Eastbrook Terminal Warehouse.*

## *New Eastbrook, A Fictive Town Along a NYC Mainline*

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*Headed by NYC E8A #4054, the 20<sup>th</sup> Century Limited is at New Eastbrook Junction.*

*Overview of the three New Eastbrook Ntrak light modules (Right). Light modules because of the different size (91.5 cm by 45cm)*





# Modeling A Small NYC Freight House

*Article by Dave Mackay*

As a member of the NYCSHS Modeling Committee, I am always looking for New York Central structures to model. I also look for pieces to complete my own HO-layout. I found a small New York Central freight station still existing in Baldwin Place, NY. This was what I was looking for. I took a trip to photograph and measure it to model "someday".



*The small NYC freight house Dave found still in place in Baldwin Place, NY. He took these photos figuring that he would want to model it in the future.*

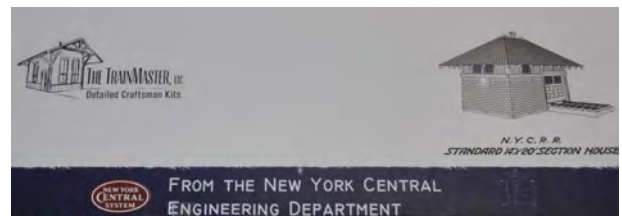


*Photos by Dave*



*A frame from a movie taken in the 1950s of the Bergenfield Freight House by an unknown individual from a moving train.*

During a meeting of the Modeling Committee, I brought the Freight Station up as a possible project. That is when I found out about the Maintenance of Way Section House kit that Manuel Duran-Duran was working on. He explained that the Section House and my Freight House were the same size, and with two NYC models so close to each other, it was probably not a good idea to make both. I had to agree, although it disappointed me. When the Section House kit came out, I purchased two, even though I only needed one for my layout.



*The NYCSHS exclusive model of the Section House kit that Dave started with for the project. (Noel Widdifield Photo)*

Last month, I had an opportunity to open the kit, and contemplate my freight house



## Modeling A Small NYC Freight House

needs. In trying to figure out what I was going to model, I found a 8mm movie video from about 1950 that was taken from the cab of a passing locomotive going right by the Bergenfield Freight House! Now I had an idea of what I needed to do. I "mocked up" a freight house with two blocks of wood (platform and building) and a cardboard hip roof. The platform appeared to be surrounded by vertical boards, and the building was set back and to the right, not centered. I could see the Section House as the Freight Station, but the platform was something else. What would that take? I decided to purchase some Northeastern scribed wood siding, and Bachmann Picket Fence. The siding would be my platform, and the picket fence my vertical boards with corner posts. When it arrived, I got to work.

I took my "mock-up" platform, and painted the edges black. I then took the Bachmann fence and cut off all the points on the tops of the pickets. The spacing was way too large for what I was trying to achieve, so I cut new styrene pickets to go between the ones on the fence. That looked much better. I painted them NYC Dark Green (the Society paint works great!), turned them upside down, and glued them to my wood block. I then measured the top, and cut two pieces of siding for the platform. I now had my platform!

Now it was time to address the Freight House itself. I opened the kit, read the directions, and looked the pieces over. The Section House is composed of wooden slatted panels on top, and shingles on the bottom. The Baldwin Place Freight House I was using as a prototype was all clapboard, but the door and windows

were all in the same places. I proceeded to fabricate the windows and doors, per the instructions. I assembled the sides to each other, and installed the corner trims, crown and base trim pieces. That left blank walls, which I then cut clapboard siding to fit. I painted the clapboard light NYC green (again NYC Society paint, bravo!) and the trim dark green.



*NYCSHS exclusive building paints available in NYC Dark and NYC Light Green. These come in 1 oz. & 2oz bottles in the NYCSHS Collinwood Shop.*

Next was the roof, which I put together as instructed. I then painted the underside dark green, weathered the roof shingles with steam loco black and dust colored paints, and put it together. I thought I was done, and was quite pleased.

It didn't look exactly as I thought it would, but I didn't know why. Then I saw a photograph of the Briarcliff Manor Freight House in the Society 2016 Calendar. My

## Modeling A Small NYC Freight House

freight house door was wrong. There were no windows in them, unlike the Section House. The clapboard siding next to the windows was also wrong, Briarcliff Manor had shingles there. I went back to my Bergenfield photograph, and it too was shingled. I removed the door trim, cut out the trim around the small panels to make them into four large panels. I had to remove the actual wall of the original model to install the door with a recess, as

the scribed siding I used for the door panels was so thick, the door would be flush with the wall, and that couldn't be. The new panels were painted light green. I then added the shingles and painted them dark green. Now I had what I wanted. A quick check with the photograph, and I was in business! I added a little weathering, and you can see the result.



*Three views of the completed freight house that Dave constructed using the NYCSHS Section House kit from The TrainMaster, LLC. Dave did an excellent job of adapting this kit for another similar structure on his HO-scale layout.*



## *The Ninth Annual St. Louis Railroad Prototype Modelers Meet*

*Article by Dennis Regan & Bob Keeler*

*Photos by Dennis*



The 2015 St. Louis Railroad Prototype Modelers Meet took place on August 7 and 8, 2015 at the Gateway Convention Center in Collinsville, IL. Long-time St. Louis-area meet organizers Lonnie Bathurst, Dan Kohlberg, Dave Roeder, Clark Propst, and John Golden welcomed co-sponsorship by the Gateway Division, National Model Railroad Association. St. Louis-area NYCSHS members Bob Keeler and Dennis Regan once again manned the NYCSHS information table and display at the meet. This was their fourth consecutive year representing the Society and the New York Central System at these meets.



*Bob (left) and Dennis (right) at the NYCSHS display tables.*

The St. Louis Railroad Prototype Modelers (St. Louis RPM) Meet continues to grow in scope, attendance, number, and quality of models displayed. Three hundred and eighty-five people attended and over 2,000 models were on display. The purpose of the event is to share and learn new prototype-focused modeling techniques and enjoy meeting fellow modelers from St. Louis, around the nation, and around the world. This year, 14 railroad historical

societies participated - one of the largest annual gatherings of railroad historical societies in the U.S. Sixty-seven exhibitors and contributors were on-hand overall, ranging from major model manufacturers, smaller specialty manufacturers, hobby toolmakers, model weathering services, prototype booksellers, to prototype photograph suppliers. And that is a rather broad generalization of the kinds of vendors and exhibitors on hand. If you model a prototype railroad or even if you don't model but are only interested in prototype railroading, a huge selection of products and information was available.



*Bob Keeler's G-scale NYC EMD GP-40 3004 (NYC class EF-30) on the NYCSHS display table.*

The St Louis RPM Meet focuses on the models as point of departure for fascinating discussions on the prototype



## *The Ninth Annual St. Louis Railroad Prototype Modelers Meet*

represented by the model, how the owning railroad employed that prototype in operations and the methods the modeler used to create the model. This is not a competitive judged event and models of all scales, gauges and eras are welcome. This includes locomotives, freight and passenger cars, structures, vehicles and other models. To give an idea of the scope of the modeling that meet participants enjoyed this year, scale models of the Eads Bridge over the Mississippi River at St. Louis and a model of a steam-powered railroad car ferry were on display.



*Bob Keeler's HO-scale B&A 4-6-6T-tank locomotive 404 (B&A class D1a) on the NYCSHS display tables. This locomotive class was built to power Boston area commuter trains.*

A sample of Meet clinics and presentations included modeling of gondola and refrigerator cars, modeling steel mill services, and a slide show on St. Louis in transition, 1962-1982. Hands-on learning stations covered topics such as model weathering techniques, airbrush tips and techniques, small-layout operations, and programming of DCC-equipped locomotives. Meet participants came from all over. Bob and Dennis informally surveyed visitors to the NYCSHS tables to see where they were from and were impressed by the responses, which included Colorado, Nebraska, Pennsylvania, Texas and West Virginia. They also heard that folks from Canada,

Australia, and New Zealand came to the meet. Beyond all the attractions outlined above, visitors might have been further enticed to attend by the model railroad layout tours in the area arranged for meet participants in the evening, the 65 x 15-foot Modutrak N-scale layout in operation at the Meet itself, and the 260 door prizes awarded at the end of the Meet.

Bob and Dennis had two tables for the NYCSHS display. They featured both volumes of the Society's "Steam Locomotives of the NYC Lines" books, examples of the HO-scale NYC section house and 19000 series plywood side caboose kits from The Collinwood Shop.



*The exclusive NYCSHS HO scale 19000 series plywood side caboose kit was displayed on the tables.*

They displayed flyers publicizing the link to the Collinwood Shop, sample issues of the *Central Headlight*, Society color brochures, new membership applications, and membership renewal forms. As the accompanying photos show, Bob also displayed his G-scale NYC EMD GP-40 3004 (NYC class EF-30), HO-scale B&A 4-6-6T tank locomotive 404 (B&A class D1a) built to power Boston area commuter

## *The Ninth Annual St. Louis Railroad Prototype Modelers Meet*

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trains, HO-scale NYC Alco RS-1 9912 (NYC class DRS-1D), a lettering diagram for NYC RS-1 locomotives from the Society files, and a newly-acquired large framed print of NYC J1e Hudson 5300.



*This is a large roster photograph of NYC class J1e 4-6-4 Hudson 5300 Bob Keeler obtained from his friends in the C&EI Historical Society. Bob promptly added this magnificent photo to our NYCSHS display.*

Bob also brought a large NYCS system map. This map was a great starting point for discussions with visitors. Bob observed that most people who stopped to talk to us model small branch lines that interchanged with larger NYCS main lines. The system map quickly allowed visitors to show how their interests meshed with the larger NYCS. For that reason, the system map should definitely be part of future NYCSHS displays.

There was a very nice selection of accurately modeled NYC prototype models placed on display by meet participants. Last year Bob and Dennis saw models of eight NYC prototypes on display. This year they saw 22 NYC models! Please see the photos accompanying this report for most of these models (Dennis gratefully acknowledges the Canada Southern website for the valuable references to NYCS freight car lot numbers used in the photo captions.)

Bob and Dennis found themselves in several conversations with modelers interested in a broad range of NYCS operations and the NYCSHS. They answered questions about the NYCS to the best of their ability. But Bob and Dennis also enjoyed learning from display visitors about aspects of the NYCS that neither of them were familiar with.

For example, they met a gentleman who models the Big Four Cincinnati to Indianapolis line. He joined the Society in January and soon bought the CD of all the back issues of the *Central Headlight* because it is a rich source of reference material. He always looks forward to getting the *NYCentral Modeler* e-zine and said the content is "just right!"

A modeler of the steam/diesel era Michigan Central era stopped by, so Bob and Dennis gave him an NYCSHS membership application forthwith and told him the Society is a modeling resource he really can't do without.

Bob and Dennis enjoyed talking at length with J.P. Barger of Reboxx Co., makers of high-quality replacement model railroad wheelsets. J.P. told us of his youth along the Mahopac Falls, NY, branch where his Dad was a wholesale lumber dealer and long-time NYCS customer. Bob and Dennis hope he can be persuaded to possibly share his memories of those days in an article in the *Central Headlight*. Bob also talked with a former NYCS engineer who very much-admired Bob's GP-40 and enjoyed reminiscing about his time at the throttle of NYC GP-40s. Currently residing in Texas, he said there is a big contingent of NYC fans down there. Bob urged him to rejoin the NYCSHS.

## *The Ninth Annual St. Louis Railroad Prototype Modelers Meet*

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A gentleman stopped at the NYCSHS tables to talk about NYC freight operations around Benton Harbor, MI. He told Bob & Dennis that there were 200-car fruit shipments originated from area orchards and multiple 0-6-0 steam switchers stationed at Benton Harbor to take care of NYC business. Bob had an enjoyable discussion with Carl Levering, a Norfolk Southern locomotive engineer who models the NYC in O-scale. John Mitchell also stopped by the NYCSHS display and told us he had joined the Society after attending last year's St Louis RPM Meet.

With so many Meet attendees from far and wide, the NYCSHS enjoyed great exposure to potential new members.

The copies of "Steam Locomotives of the New York Central Lines, Vol. 1 and Vol. 2" were prominently displayed on the door prize table in the last hours of the meet. Lonnie Bathurst, one of the meet organizers, personally conveyed his thanks to the Society for these very generous door prize donations.

Bob & Dennis are enthusiastic about gaining potential new and renewal Society members from this year's St. Louis RPM Meet. Visitors took four new membership applications without prompting, so there is definite interest and enthusiasm out there among Meet attendees.

Bob & Dennis enjoyed displaying the accurate NYC prototype models from The Collinwood Shop at our display. This was a first since the NYCSHS began participating in these St Louis RPM Meets four years ago. They hope that this kind of opportunity for visitors to see the actual

items will stimulate follow-up Collinwood Shop sales. Bob and Dennis are excited by news that Society leadership is actively considering creating color NYC-themed coverings for the NYCSHS display tables. This will add a great degree of professional-level visual impact. Bob and Dennis will be looking into locally-producing attractive color business cards to hand out which would include key NYCSHS contact information and keep that information visible in the recipient's "take-aways" from the meet. They obtained several cards from other historical society representatives and those should provide some design concepts that could be adapted for NYCSHS cards.

This year's St. Louis RPM Meet featured a first-ever meeting of representatives of all participating railroad historical societies. The purpose of the meeting was to gather leadership and representatives from the historical societies to discuss the future of railroad historical societies in general, how to work together toward common goals, and how to maintain and increase historical society membership and participation. This meeting was closed to all but historical society representatives. Dennis represented the NYCSHS. Topics included:

- 1) Membership - how to keep members and attract new ones.
- 2) Publications - what are people looking for?
- 3) Archives - do you have an archive, are you digitizing material, and what are your long-term plans?
- 4) Marketing and Sales: - what works?

In advance of the Meet, Noel Widdifield provided Dennis with a talking paper that



## *The Ninth Annual St. Louis Railroad Prototype Modelers Meet*

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he used to address these topics. Formal minutes of the meeting were to be compiled after-the-fact by the gentlemen who called the meeting. What follows is an informal brief summary of some of the discussions based on Dennis's notes:

1) The rep from one society mentioned that their group initially felt that catering to modeling interests might cut into membership. The larger group of discussion participants voiced opinions and evidence that might counter that conclusion.

2) One society rep stated that his group concluded that their web-based modeling magazine should be limited to access only by society members. This society felt "why should we give it away for free?" when it could be a tool to attract readers to formally join the society.

3) There seemed to be a consensus among those present that the society main publication should be kept primarily prototype-only, with modeling content kept in a separate publication or at least not the principal emphasis of main publication articles. The separate modeling publication could be web-based. Some societies do not make a complete separation of prototype and modeling content in their main publication. Instead they present prototype articles in that publication and then follow that content with advice on how to model the prototype subjects. Most present agreed that a modeling-only main publication would not be a good approach because then the society would lose members who are not modelers.

4) The question was asked of those societies that place ads in modeling publications as to how many new members they gained from those ads. How successful are those ads as a recruiting tool? This was a practical question particularly for those societies that may not have a deep treasury from which to pay for such ads. The Railway & Locomotive Historical Society asks all new members who are joining where they heard about the society.

5) Another questioner asked whether paid ads mentioned the modeling projects that the society placing the ads is working on. This approach should attract potential members who would need those models.

6) The Missouri Pacific Historical Society reaches out to railroad meets across the U.S., Amtrak stations, and even to model railroad kit manufacturers to ask permission for the society to place membership brochures with those entities.

7) There was widespread agreement that the main publication of any society is the public "face" of that society for the majority of their members. It needs to be issued in a timely manner or else the membership feels "why waste my money if I get nothing for my dues?" Some horror stories were shared about what happened to more than one society when their main publication became unreliable.

8) The Pennsylvania RR Technical and Historical Society rep said that they individually ask each and every member who quits their society why that person left. Then they apply the feedback to fix the problem. They also send an email welcome to each new member that

## *The Ninth Annual St. Louis Railroad Prototype Modelers Meet*

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confirms their membership. Members are made to feel welcome and are made aware when they will receive their first mailing of the society publication. Apparently in at least one other society, past practice left new members wondering if their membership application was even accepted and had to endure a long wait time before they got their initial publication.

9) The preceding topic led to some interesting discussion on the pros and cons of starting the society membership year on January 1<sup>st</sup> or within the quarter of receipt of application or making it effective the date the application is received. There are genuine administrative issues that come from each option that have to be considered since the administrators are volunteers, so the system has to be manageable. These options also affect how quickly new members get the first copy of the society publication in their hands. Long lag time from sending in one's money before one gets their magazine does not bode well for the society.

10) It can be a headache to sell photos from society archives because of potential copyright issues. Discussion showed that copyright law is not uniformly interpreted among historical societies. This led to discussion of the benefit that might come from forming an association of all railroad historical societies so that the association could provide services that the societies might need, such as professional legal advice on copyright law implications. Attendees seemed to agree that there should be cooperation among the societies going forward.

11) White River Productions, publisher of several societies' magazines, mentioned that they are in the advanced stages of fielding a digital magazine platform that would not only provide PDFs of magazines but also formats such as Android apps which should be especially attractive to younger potential historical society members. White River mentioned this initiative as an example of a possible cooperative effort for societies. They could offer this kind of production service to any society, thereby saving the society from having to develop their own digital publishing programs.

Discussions of the type mentioned here would have continued further, but the time allotted for this special meeting was completely used up. Participants thanked the meeting organizers, generally agreed that the session was productive and look forward to continued meetings of this type.

Bob & Dennis again thank NYCSHS directors Dave Mackay, Noel Widdifield and Jim Suhs for enthusiastically supporting their participation. Bob & Dennis look forward to increased interest in the Society and gaining enthusiastic new members. They encourage all NYCSHS members to come out for the railroad prototype information sharing and camaraderie that are the hallmarks of these St Louis RPM Meets and those held elsewhere. And if you do attend a meet, why not go ahead and staff a display table (or two) there to get the word out about the NYCSHS to prospective members!



# The Ninth Annual St. Louis Railroad Prototype Modelers Meet

## NYCS Models Displayed at the 2015 St. Louis RPM



HO-scale model of NYC boxcar 153739 (NYC Lot 764-B built by Aaron Gjermundson of Taylor, ND.



HO-scale model of NYC boxcar 166769 (NYC lot 764-B) built by Stuart Thayer of Georgetown, KY.



HO-scale model of NYC Budd 10 roomette-6 double bedroom Pullman "Peaceful Valley" built by Michael Ferris.



HO-scale model of NYC Budd 56-seat coach 2567 by Michael Ferris.



HO-scale model of NYC Pullman-Standard 64-seat coach 3024 by Michael Ferris.



HO-scale NYC EMD GP-7 5756 (NYC ERS-15) lightning stripes, by Michael Ferris.



HO-scale model of NYC Fairbanks Morse CFA-20-4 5008 (NYC class DFA-6A) built by Michael Ferris.



HO-scale model of NYC 70-ton flatcar 499684 (NYC lot 712-F) built by John Golden.



HO-scale model of NYC express car 9291 (former troop sleeper) built by John Golden.



# The Ninth Annual St. Louis Railroad Prototype Modelers Meet



*HO-scale model of NYC hopper car 837004 (NYC lot 410-H) built by John Golden.*



*HO-scale model of NYC hopper car 861345 (NYC lot 533-H) built by John Golden.*



*HO-scale model of NYC 50-ft. mill gondola 751943 (NYC lot 591-G) built by John Golden.*



*HO-scale model of NYC 50-ft. RBL insulated boxcar 78988 assigned to General Foods service (NYC lot 922-B) built by David Lehlbach of Asheville, NC.*



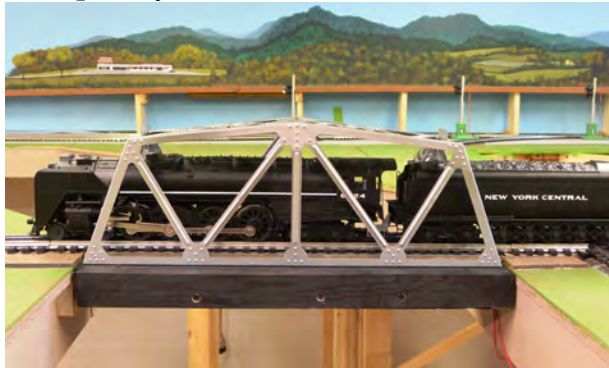
*HO-scale model of NYC Pullman-Standard 6 double bedroom-buffet-lounge Pullman 10615 under construction by Michael Ferris.*



# Building a Simple O-Scale Lift-Out Bridge

Article by Bob Shaw

Part 4 - NYC Water Level Route Layout Update ... Building a simple O-Scale lift-out bridge that aligns perfectly every time and quickly



*A massive Lionel NYC Niagara speeds across a new lift-out bridge that Bob built on his NYC Water Level Route layout. The bridge is strong and perfectly aligns every time when placed back into position.*

In contrast to publications that present a comprehensive view of model railroad layout design and construction, this article recaps the process used to build my latest O-gauge layout, along with tips gained from constructing previous ones during the past 30 years. Hopefully, sharing my successes and mistakes will help make your layout building experience more enjoyable.



*Bob's new lift-out bridge connects both sides of his layout, offers easy access to an open center aisle, and controls a micro switch that protects a train from running off the table when the bridge isn't in place.*

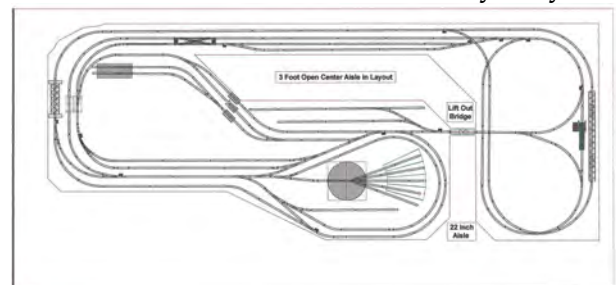
Photos by Bob

## Introduction - Project Goals

Part 4 in our series continues to track construction of my O-scale layout that interprets the scenic NYC Water Level Route between New York City and Chicago. This installment reviews how to build a simple lift-out bridge that ensures that GarGraves Track on the bridge aligns perfectly with track on the layout every time when the bridge is put back in place after being removed. Furthermore, the process only takes a few seconds! Because smooth operation takes priority over "realism" on my layout, I didn't strive for a prototypical bridge design, but rather for one that would be simple to build and would operate effectively under constant use. Another goal was to prevent a train from accidentally running off the layout when the lift-out bridge wasn't in place. While this bridge is only 18.50 inches, the design should work well for bridge spans of three feet or more.

## Lift-Out Bridge Challenges and Construction

While a narrow 22-inch aisle permits access to the 3-foot open center of my 35.75 x 14 foot layout, it interrupts the track plan and requires a bridge to span the gap to connect the two sides of my layout.



*The lift-out bridge, access aisle it spans, and open center aisle are shown in a CAD drawing of the 35.75 x 14 foot layout.*

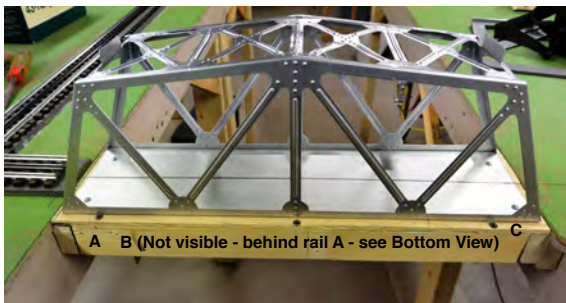
# Building a Simple O-Scale Lift-Out Bridge

After reviewing a number of designs, I chose a simple lift-out bridge over a more complex hinged lift-up version that's designed to handle much longer spans. Besides, I had a metal bridge that was perfectly sized for the job and the lumber needed to make it.



*The lift-out bridge spans a 22-inch aisle, and rests on mounting blocks affixed to horizontal bench work rails on each side of the aisle.*

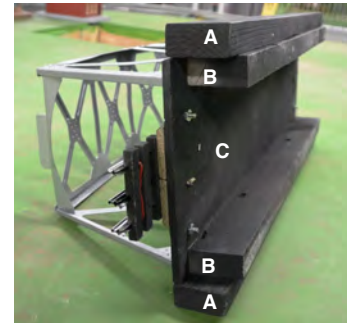
The plan called for securing the metal bridge to a wood platform, which would rest on top of a wood block mounted on the benchwork frame on each side of the aisle. Note that the bridge platform is screwed to the top of the side rails using counter-sunk dry wall screws, which are also used to secure the sides.



**Lift Out Bridge Platform Dimensions (Inches) - Top View**  
A - 2 Pine Outer Rails - 18.50L x 0.75W x 1.75H  
B - 2 Pine Inner Rails - 16.75L x 0.75W x 1.75 H (not visible)  
C - 1 Luan Bridge Platform - 18.50L x 5.50W x 0.25H

*Bridge dimensions are presented in a side view of the bridge during construction. Note how the bridge platform rests on top of the mounting blocks, which can be seen because the outer rails haven't been installed yet.*

The bridge is secured to the top with machine screws and nuts that drop through holes drilled with a metal bit. Another perspective of the bridge platform is shown in this bottom view.



**Lift Out Bridge Platform Dimensions (Inches) - Bottom View**  
A - 2 Pine Outer Rails - 18.50L x 0.75W x 1.75H  
B - 2 Pine Inner Rails - 16.75L x 0.75W x 1.75 H  
C - 1 Luan Bridge Platform - 18.50L x 5.50W x 0.25H

*A bottom view of the completed bridge clearly shows the two outer and two inner rails, as well as the platform with the bridge, roadbed, and track screwed in place.*

The bridge platform is constructed from five pieces of lumber:

- Top - A one-quarter inch luan platform rests on top of inner rails (B), which is one-quarter inch lower than outer rails (A). This sandwiches the luan between the outer rails (A) resulting in a flat surface.
- 2 outer rails (A) - See images 6 and 7 for dimensions.
- 2 inner rails (B) - See images 6 and 7 for dimensions. Image 6 was taken before installation of the two longer outer rails (A). It shows these two inner rails (B) are shorter than the outer rails by 0.75 inches at each end, so they will fit snugly over the 0.75-inch wood blocks mounted on the benchwork frame on each side of the aisle to insure proper alignment.



# Building a Simple O-Scale Lift-Out Bridge

A close-up of each block can be seen in these images.

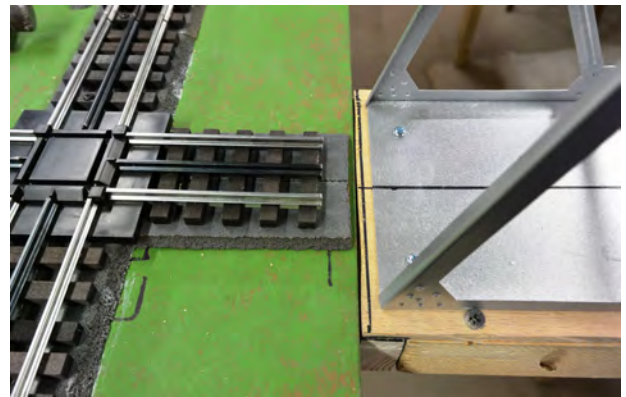


*Mounting blocks on each side of the aisle follow a centerline to insure proper alignment when the bridge is placed on them. They need to be carefully positioned vertically so the bridge track aligns with the layout track when the bridge is placed on them.*

## Aligning the Bridge on the Mounting Blocks

Aligning a lift-out bridge so the tracks on one side of the layout perfectly match tracks on the other side is the major construction challenge. The first step is to draw a centerline down the middle of the

bridge so it matches the centerline on each mounting block.



*A centerline was marked on the metal bridge, which was aligned with centerlines on the mounting blocks.*

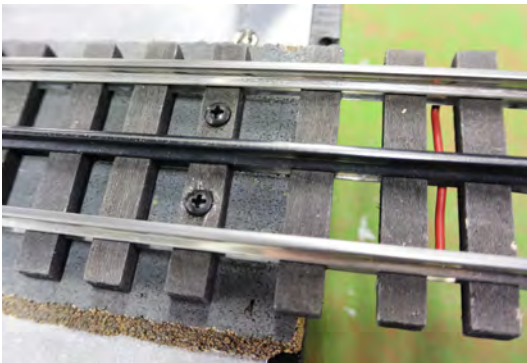
Next glue cork roadbed so it straddles the bridge centerline and lay a continuous run of Gargraves track on top of the bridge roadbed, so it connects to track on each side of the gap.



*With the lift-out bridge resting on the mounting blocks, a continuous span of cork roadbed and track is installed on one side of the layout, run across the bridge, and connected to track on the other side.*

# Building a Simple O-Scale Lift-Out Bridge

With the liftout bridge in place resting on the blocks, screw the track going over the bridge into place on the bridge using GarGraves track screws. To penetrate the cork bed and metal bridge, a small metal bit was used to drill pilot holes, so track could be screwed to the luan top platform.



*The roadbed and track are screwed to the bridge platform to ensure perfect alignment of the track.*

Then I cut all three track rails on each side of the aisle with a cutting disc mounted in a Dremel® tool, which left a section of track mounted on the bridge that precisely matched the gap.



*After screwing the roadbed and track to the bridge, all three rails were cut with a disc mounted in a Dremel® Tool, which left a section of track on the bridge that precisely matched layout track on each side of the bridge.*

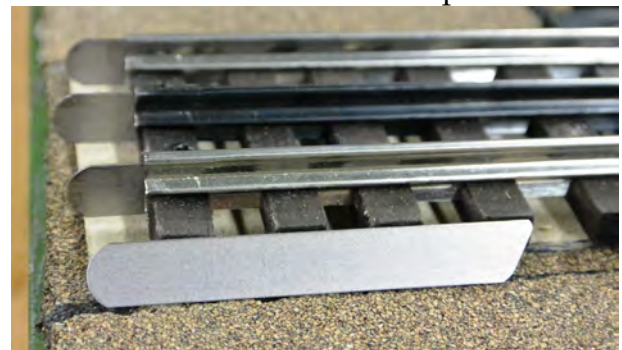
## The Key to Perfect Track Alignment

GarGraves 910-4 Strip Track Connectors, or "Fit-Up Blades", are the secret to perfect alignment of track on the layout with track on the lift-out bridge. I used these blades previously to close the gap between two opposing tracks to add a section of track to track. This was installed to avoid having to take up the existing track to have enough room to insert track pins in the end rails. Then I decided to use them on the lift-out bridge. Here's how they work.



*GarGraves Strip Track Connectors, or "Fit-up Blades" are the key to perfect alignment of each track rail when the lift-out bridge is put in place.*

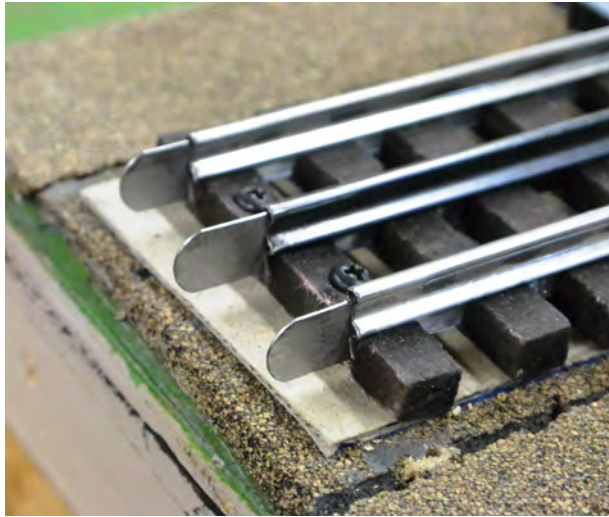
Each thin blade is designed with a pointed end that can be inserted into the vertical slot in each rail until 0.25 inch protrudes.



*Thin GarGraves "Fit-Up Blades" are designed with a pointed end, as shown in the single blade resting against the ties that's inserted into the vertical slot in each track rail.*



## *Building a Simple O-Scale Lift-Out Bridge*



*Fully inserted GarGraves Fit-Up Blades protrude 0.25 inch at the end of each rail. They were inserted into track rails on each side of the lift-out bridge to secure the bridge.*

I inserted these into the layout track rails on each side of the bridge. When the bridge was lowered onto these blades and pushed down into place, the layout track and bridge track aligned perfectly.



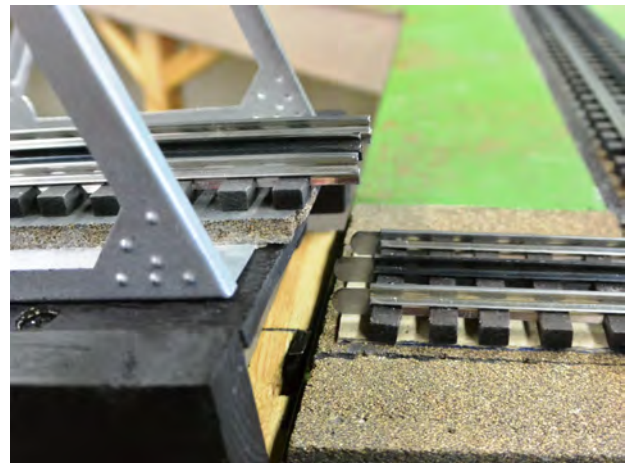
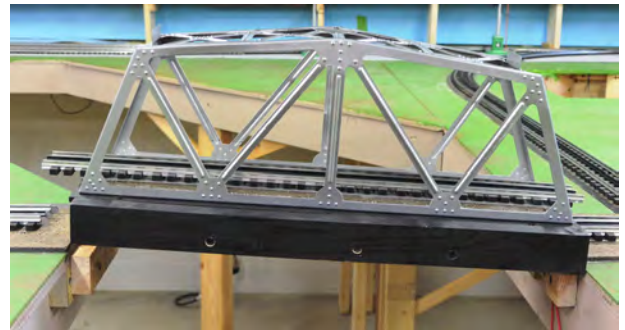
*This close-up of the bridge being lowered onto the Fit-up Blades shows how slots in the upper track rails exactly fit onto each blade.*

Excellent electrical conductivity is maintained because the track fit together snugly. Separating the track to remove the bridge is as simple as pulling up on

each end of bridge to disengage the bridge track from the layout track.



*When the lift-out bridge is completely installed, track on the bridge is snugly mated to layout track resulting in smooth track transition and excellent electrical conductivity.*



*Note how the bridge platform ends fit snugly over the mounting blocks to align both sides, while the three track rails are aligned by being pushed down onto the fit-up blades.*



## *Building a Simple O-Scale Lift-Out Bridge*

Repositioning the lift-out bridge is completed by aligning the track centers over the fit-up blades and pushing down until the track tops are flat.



*With the track ends pushed down until they are flat, the lift-out bridge is fully installed and operational. The entire repositioning process was completed in a few seconds.*

### **Protect Your Trains When the Bridge Isn't in Place**

While it's easy to say we'll always remember to replace the lift-out bridge before running trains, it's easy to forget to do it. That's why I invested a few dollars in a micro switch. It protects one of my valuable trains from running off the table because the micro switch shuts-off power to the center track on each side of the bridge when the bridge is not in place. Here's how it works.

I purchased a Radio Shack SL-2NW1 micro switch that is in the "normally off" position until it's turned-on by pressure being applied to the top lever. I installed it on the benchwork side rail at a height that it would be turned-on by the weight of the lift-out bridge when it was placed between the two mounting blocks.



*A micro switch was installed to protect a train from running off the layout when the bridge is removed. It's turned "on" by the weight of the bridge depressing a lever on the top of the micro switch, and it's turned "off" when the bridge is removed, which releases the lever.*

The wood mounting block serves three uses. First, the back of the block was carved out so it fits over the micro switch to protect it from being scraped as people traverse the narrow aisle to the layout open center space.



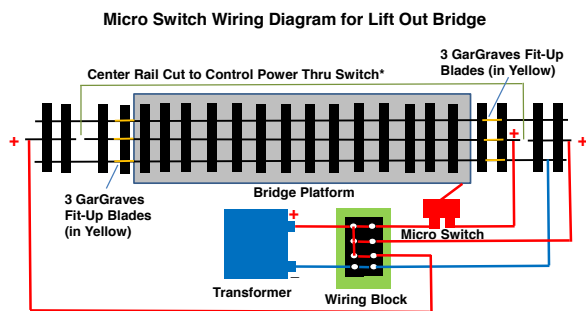
*This mounting block was hollowed out in the back so it can cover and protect the micro switch. It also serves as a ledge for the lift-out bridge to rest on. The weight of the bridge depresses the micro switch lever to supply power to the center rail of the track.*

# Building a Simple O-Scale Lift-Out Bridge

Second, the block serves as a ledge that perfectly positions the lift-out bridge when it's placed over it. Third, the weight of the bridge presses down on the protruding micro switch lever, so it's in the "on" position to supply power to the bridge.

Two micro switch wires are also shown here. Wire to one terminal on the micro switch connects to positive (+) transformer power on the wiring block on the lower right, while the other wire runs under the benchwork and connects to the center rail on the isolated track section. The wiring block also provides negative (-) ground to the outer rails through wires connected to the (-) terminal.

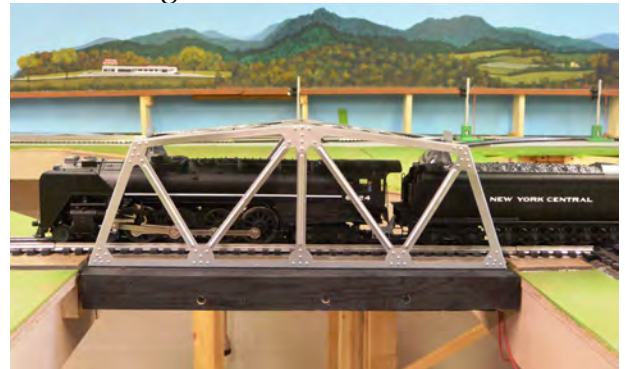
Wiring is straight forward as the micro switch is a simple "on" / "off" switch that's "off" when the lever is raised and "on" when the lever is depressed flat. Be sure to leave train-stopping room by cutting the center rail at 2.5 feet back from the lift-out bridge on both sides of the layout.



\* Cut track center rails at least 2.5 feet back from the Lift Out Bridge on both sides

*Wiring the micro switch is easy, as it's a simple "on"/"off" switch. Be sure to cut the track center rail at least 2.5 feet back from the edge to leave sufficient "stopping room" for a train after it runs into the unpowered track.*

While the liftout bridge needs some finishing aesthetic touches, it performs flawlessly enabling safe train passage over the aisle from one side of my layout to the other, even for large engines such as this Lionel Niagara.



*Bob's new lift-out bridge works flawlessly for all of his motive power, even when under heavy loads from large locos like this Lionel Niagara.*

Thanks for your interest in this article. Part 5 will continue to cover construction of my NYC Water Level Route layout including wiring a double-crossover and other ideas that you might want to incorporate into your layout.



*This is another photo of a WrightTrak caboose resin model constructed for one of our members by Erik at SPR Backshops. Available service in the Collinwood Shop.*

# *Pacific Limited O-Scale Boxcars (USRA Variants)*

*Article by Brad Andonian*

*Photos by Brad*

Pacific Limited was created by Pat O'Boyle in the 1980's as a vehicle to import O scale brass trains. Mr. Carlo Rossi of San Francisco was intimately involved in assisting Pat with financing and serving as a sounding board for models to produce. Pat had spent time as an officer in the Oakland police department.



However a disability allowed him flexibility to work on the import side. Prior to becoming a police officer, he served as an officer in the US Army. As he had an affinity for model trains and saw an opportunity, he began producing models in Korea. A stickler for detail, he produced exacting variations with attention to roof, ends, underbody, door and brake detail. One of the more intriguing aspects was that none of his models were imported painted—although he did seek to offer/include CDS transfers with models. Only a few early models came with O-scale Bettendorf trucks. Pat saw providing trucks as an issue, as his customers were both P:48 and O-scalers. The added expense of having incorrect trucks was an issue he wished to avoid.

The New York Central Series was his last series to be imported over the 1999-2000 period. Pat utilized the knowledge and

advice of Mr. H. Lansing (Lans) Vail, a noted NYC historian, to insure prototype information was correct.

Earlier Pat made a run of NYC/MDT cars that I plan to cover in a future article. While exact details of production are not available, thirteen model variants were produced. I have seen one model file courtesy of M. David Vaughn. David purchased Pat's papers and the Pacific Limited name from his Estate.

Based on my examination of Pat's production notes of another model, I have concluded that around 250-350 models in all versions were produced. Several models have label notes stating a quantity of twenty pieces of each was created [PL-1900 Michigan Central and PL-1550 Hannibal Connecting]. My examination has led me to understand the only difference between the Hannibal Connecting and Northampton and Bath car is a welded plate on the panel door.

An exciting aspect of these cars to those who model in O-scale is the wonderful array of door, car ends, and roof designs—and that the subsidiary lines were covered. Most of the versions are scale height of 8'7" per the prototype. There are a few 9'4" and one 10' version.

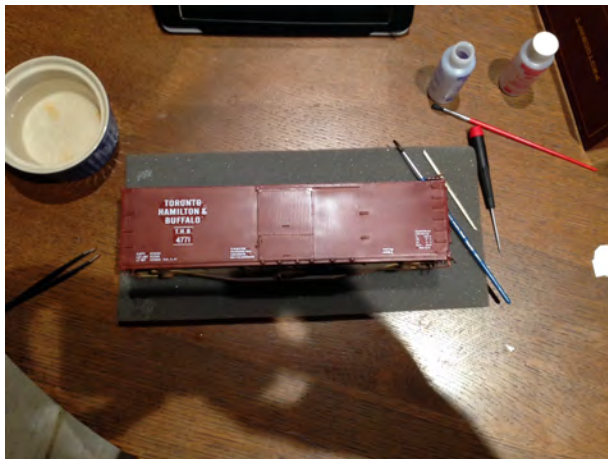
## **Preparation to Paint**

As these models are new and most never handled, I begin with a soak in acetone to break down any oils and or lacquer. I use a brush to insure that any dirt, grime, or lacquer is eliminated. Cars are then sand blasted, washed in soapy water [dawn], then left to dry. Decals were produced



# Pacific Limited O-Scale Boxcars (USRA Variants)

and printed by Rick Leach. Rick Leach created most of the decals for the series including a custom Big Four decal. He added correct reweigh and COTUS data to the sheets--- a challenge to read and apply!



Paint is from Tru-color as they have matched shade cards to render accurate colors. As many of you know, *Railroad Prototype Cyclopedia* issue 21 has a lengthy and accurate article on these cars. I will strive in the issues ahead to offer more models that accurately represent the NYC and their subsidiary lines in O-scale brass.

## Pacific Limited O-Scale Boxcar Model Numbers and notes:

- PL-900 USRA double sheathed
- PL-1400 NYC (Big Four, Boston & Albany, Cincinnati Northern, Michigan Central, Peoria & Eastern and NYC) Gilroy doors, KC brakes
- PL-1450 (NYC, B&A, P&E) 8'7" Youngstown Door with AB brakes
- PL-1500 Northampton & Bath 8'7" Creco Door
- PL-1550 Hannibal Connecting 8'7" Creco Door
- PL-1600 (NYC and P&E) 9'4" Youngstown Doors AB brakes Dreadnaught Ends
- PL-1900 Michigan Central 8'7" Gilroy Door (1 1/2) KC brakes
- PL-2100 NYC autocar 10' Gilroy Door AB brakes and extended roof
- PL-2150 (NYC, P&E, B&A) 8'7" rebuild 1939/40 Youngstown door with rectangular panel roof and wood running board
- PL-2200 (NYC, B&A, P&E) 8'7" Youngstown doors, AB brake, Apex roofwalk, rectangular panel roof c.1940-50 era
- PL-2400 (NYC & MC autocar) 9'4" Gilroy Door, KC brakes
- PL-2450 (Michigan Central autocar) 1 1/2 Youngstown door KC brakes
- PL-2500 (NYC autocar) 9'4" Gilroy door AB brakes
- PL-2550 (NYC autocar) 9'4" Youngstown Door AB brakes
- PL-4450 (NYC autocar) 8'7" Diagonal Panel Roof, Apex Roofwalk 1950 rebuild

## *Pacific Limited O-Scale Boxcars (USRA Variants)*



*Brad has a large collection of these Pacific Limited models and will be sharing them with us in future articles. These fine brass models have been around for a long time, and it is wonderful that Brad is taking the time and talent to paint and letter them.*

*These models were purchased from the NYCSHS. The next article will be NYC gondolas with an emphasis on the models with cement containers. He purchased two Bob Parri kits when he made the large purchase of these surplus models from us. He just got the models built this summer and is building and painting his way through them. Thirty gondola kits were made and he has three of them.*

*Watch for future articles from Brad.*





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Model by Noel Widdifield, NYCSHS Member

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Our model selection includes NYCS locos, rolling stock, and buildings, and they all are at 20% off MSRP. For some of the locos this means a savings of more than \$50 each.

To take advantage of this 20% savings, you must join today. Memberships start at only \$39 per year and bring many benefits in addition to the 20% savings in the "Collinwood Shop". Join using the application on page 60 or [Click here to join at the "Collinwood Shop"](#).

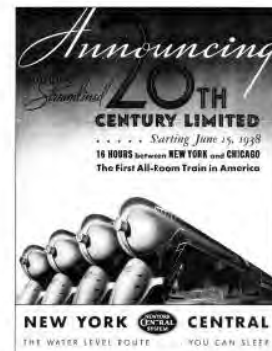


# New York Central System Historical Society, Inc.

P. O. Box 130, Gates Mills, OH 44040-0130

## 2016 New Membership Application Form

Regular Member – United States	\$39
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Digital Edition of <i>Central Headlight</i> (Additional)	Add \$15 _____
We are offering this Digital Edition as a supplement to the print version. You must agree not to provide this to others by signing here:	Total \$ _____



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Membership runs from January 1 to December 31.

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Modeling Interest: Do you model the NYCS? \_\_\_\_\_ What scale? \_\_\_\_\_

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# New York Central System Historical Society, Inc.

P. O. Box 130, Gates Mills, OH 44040-0130

## 2016 Membership Renewal Form

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		Membership \$
Options: - Contributing Member*	add \$11.00	<input type="checkbox"/>
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\*Those members who wish to render greater support to the work of the Society, and these generous contributors will be identified in *Central Headlight*. The Society no longer offers new life memberships – life categories are for existing Life Members only.

Membership runs from January 1 to December 31.

You may also join by going to our store at [www.NYCSHS.net](http://www.NYCSHS.net) or our website is [www.NYCHS.org](http://www.NYCHS.org)

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# *New York Central's Fifty-Foot PS-1 Boxcars*

## *Part 2: Lot 136-B NYC #s 215000 - 215399*

*Article by Seth Lakin*

*Photos by Seth*



*Follow along and see how to rebuild this model of a NYC rebuilt boxcar.*

In this two part series we are examining New York Central System's two groups of 50-foot PS-1 boxcars. In part one, which appeared in the 4<sup>th</sup> Quarter 2015 issue of NYCentral Modeler, we looked at the 100 boxcars that were delivered new to NYC subsidiary Peoria & Eastern. In this second part we will look at the 400 boxcars that were rebuilt in NYC's own shops from 40-foot cars and how to model them in HO-scale.

Since the early 1900's and well into the 1950's, the 40-foot long boxcar was the king of freight cars. During the 1950's, the 50-foot boxcar became the king of the North American freight car fleet. Faced with an abundance of post WWII built 40-foot boxcars that were no longer in high demand, the NYC rebuilt many into 50-foot long cars. Beginning in 1965 and continuing up to the Penn Central merger in 1968, a total of 3,200 stretched boxcars emerged from NYC's Beech Grove, IN shop. Three different lots were out shopped with each having different

characteristics. Lots 968-B (2000 cars) and 136-B (400 cars) started with six-foot single doors and emerged with 10-foot single doors, while lot 992-B (800 cars) retained their original 15-foot six-inch double doors.

The 40-foot boxcar-stretching program continued into the Penn Central era as well. Six hundred cars were added to lot 968-B, and 850 cars were out-shopped as lot 159-B. The 1948-built PS-1's from NYC 167000-167999 lot 765-B and 1947-built New Haven PS-1's from NH 33500-34999 were used for the lot 159-B rebuild. These cars were numbered PC 165500-165949 and PC 264000-264399.

During 1967, the rebuilding of 15-year old 40-foot Pullman Standard PS-1 boxcars into 50-footers took place. These cars were originally built in 1952 with assigned numbers NYC 180000-180499 and lot number 842-B. They were standard 55-ton capacity with 10-foot six-inch interior height. Exterior details included six-foot wide Superior doors, ASF A-3 Ride Control trucks, and Miner hand-brake equipment. The rebuilding process included cutting the car in half and adding a 10-foot section to the center of the car. Other changes included new 10-foot doors, removal of the roof walk, and lowering of the end/side ladders and brake gear. Other changes included rebuilding of the side sill and adding reinforcing braces around the bottom of the door. The 400 rebuilt boxcars that emerged from the shop were numbered NYC 215000-215399 and assigned to lot 136-B.

# *New York Central's Fifty-Foot PS-1 Boxcars*

## *Part 2: Lot 136-B NYC #s 215000 - 215399*

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*NYC 215259 lot 136-B poses for its builder's photo. (NYCSHS Collection)*

Following the Penn Central merger in 1968, the lot 136-B cars were assigned to PC 163000-163397 series. Two cars were not included in the PC numbering series; NYC 215157 and 215247 were likely destroyed before the merger. Upon the formation of Conrail in 1976, 251 of the 400 still retained their NYC reporting marks while 53 had been renumbered into the given PC numbers. The cars that had retained the NYC numbers were assigned numbers between CR 160989-CR 161423. These were dovetailed into open spots in PC lot 968-B cars that had kept their PC numbers under the Conrail renumbering plan. The 53 cars that were renumbered into the PC 163000-163397 series retained the same numbers: CR 163000-163397 under the Conrail plan.

This project almost stopped even before it had started. I ordered an Intermountain #40999 50-foot single door PS-1 for this project. This model has been around for quite a few years. When the kit that came across the hobby shop counter; it was not in the familiar red and white cardboard box but one of black and red with a window to show the car off. This car was

made in China and not the familiar one made in Colorado. In the past all Intermountain kits have had drilling dimples located on the underside of the roof for installation of the roof walk. The modeler could choose not to drill the holes and easily model a car without a roof walk. This car had the roof walk mounting pin holes already drilled out. Faced with plugging, sanding, and contouring the holes on my model, it was almost certain to join my pile of other projects that have been started but will never be finished. A fellow modeler had some older Intermountain PS-1 kits squirreled away and a plea went out to swap roofs with me. Now, with a roof without holes, I was full steam ahead.

In describing how I built my model of the 136-B boxcar, I am going to break the modeling into the following sections: roof, ends, doors, body, paint and decals, and final assembly.



*Conrail 162056 formerly NYC 215206 is at Altoona, PA in June 1976. Notice how the roof support brackets are missing from the center of the car. (Lynn Roberts photo courtesy FallenFlags.org)*

### **Roof**

The only modifications to the roof are in details. On the five seams between roof panels on the outside of each end, I added Plano boxcar roof brackets. There are no brackets added to the center 4 panel seams.

# *New York Central's Fifty-Foot PS-1 Boxcars*

## *Part 2: Lot 136-B NYC #s 215000 - 215399*

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*The only modification to the roof was the addition of Plano roof brackets.*

### **Ends**

To both ends, the end ladders were trimmed above the center-mounting pin, a Detail Associates eyebolt was added for the coupler pin lever that will be installed during final assembly; and the kit's grab irons and tack boards were installed. A handrail across the end was formed from .019 brass wire. A DA eyebolt was threaded through the wire and attached to the center of the end, while a short piece of .010x.040 styrene was added to the right end of this handrail to represent the bracket that attaches it to the end. Then I decaled a strip of Archer rivets to the panel seam to backdate the end to an earlier production than the modeled end depicts. On the B end, mounting holes for the brake gear and brake step were plugged with .025-styrene rod. Moloco Miner handbrake housing with a Kadee Miner brake wheel was added at the lowered location. Detail Associates 6402 is an alternate to these two parts but was unavailable at the time I needed them. The kit's brake step and brackets were also relocated.

### **Frame and Side Sill**

NYCSHS drawing N-59355 shows the rebuilt frame and is included with this

article. To start on replicating the rebuilt side sill and frame, remove the entire side sill from the body of the model. The prototype's side sill is a ten-inch C channel.



*The completed end prior to painting shows the lowered brake wheel, end ladder, and brake step, as well as, a rivet strip that connects the top and bottom halves of the end.*

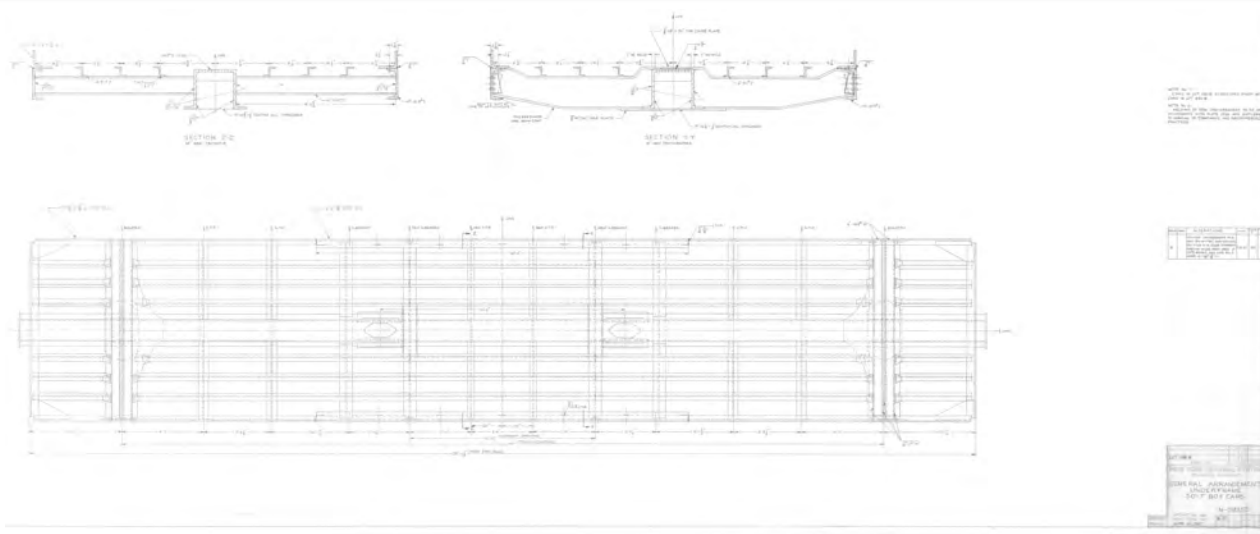
To replicate the rebuilt sill a piece of .100" styrene channel was cut to fit between the bolsters. Notches in the top lip of the channel allow it to fit around the truck bolsters. The ends of the channel have an angled notch to allow the trucks to swing.

I usually do not modify the under frames of my models. Detailing is limited to what is included in the kits. However the deep cross members of the 50-foot frame hung below the shallower new side sill, so modifications had to be made.



# *New York Central's Fifty-Foot PS-1 Boxcars*

## *Part 2: Lot 136-B NYC #s 215000 - 215399*



*An Intermountain 40-foot PS-1 and a printed copy of the NYCS drawing help in constructing the under frame.*

The prototype's frame cross members are made up of existing cross ties and cross bearers from the original frame and new cross ties and cross bearers from the new frame section. They are arranged in the following order starting at the bolster, existing cross tie, existing cross tie, existing cross bearer, new cross bearer, new cross tie, new cross tie, new cross bearer, existing cross barer, existing cross tie, existing cross tie, and ending at the opposite bolster.

None of the cross members on the 50-model model's frame match the prototype. I had pondered using an Intermountain 40-foot PS-1 frame and adding in a new section but decided that using the kit's center sill with scratch built cross members would not be too difficult. So I removed every cross members between the bolsters and started on reconstruction of the frame. The prototype existing cross bearers have a Z-shaped profile. The top part of these will be against the bottom of the floor and not be seen. I made these from a piece of

.020 x .060 base with a piece of .020 x .030 on top making a step piece. The new cross brace is simply an 8-inch I-beam, which is replicated by a .080" I-beam. Both the existing and new cross bearers are made from a vertical piece of .040x.100 then a .020x.080 cap was added. The difference between the existing and new cross bearers is that the existing cross bearers have a long taper while the new cross bearers stay at full height then have a short steep taper near the end. Then a short piece of .020x.080 was added as a tie between each half across the center sill.



*The model's under frame cross members are made up from styrene strips and I-beams.*

While still on the frame, the kit's brake equipment is installed. Also to properly mount Kadée's HGC trucks, the bolster

# *New York Central's Fifty-Foot PS-1 Boxcars*

## *Part 2: Lot 136-B NYC #s 215000 - 215399*

must be flat. Trim the raised portion of the bolster where the trucks mount so the bolster is flat.

### **Doors**

The Intermountain kit has a 9-foot door opening while the 136-B has a 10-foot opening. Kadee and Accurail both have a 10-foot Youngstown door that could work; neither is truly correct. The corrugations on the prototype door do not go all the way to the door edge leaving about 6 inches on either side of the door that is flat. The Kadee and Accurail 10-foot doors have the corrugations go to the end of the doors. So looking through my parts drawer that contains 100's boxcar doors, I came up with a solution. I started with a pair of Kadee 9-foot Youngstown doors and kitbashed them to achieve the desired door. Kadee's 9-foot doors are not available as a separate part. However, I was able to purchase them directly from Kadee for a very reasonable amount. To begin the door modification I cut off clips from the inside to have a flat back to work with. Then I cut the small lip on the right side of the door. I glued a piece of .040x.080 styrene to a piece of .010x.100 styrene to make a .050x.080 piece with a .010x.020 lip on the side. This was glued to the right side of the door. To the left side of the door, I glued a piece of .040x.080 on top of a piece of .010x.080 to make a piece .050x.080. Then this was glued to the right side of the door. I was hesitant if this would work or not, but when paint was applied I was happy with the results.



*The car's doors were modified by adding styrene strips to the sides.*

### **Body**

All the vertical weld lines, the vertical doorstop to the left of the door, and the doorstop at the right end of the track were all chiseled off then sanded smooth. The horizontal weld lines near the roofline and bottom sill as well as the top door track and doorstop were all retained. New weld lines were scribed using a modeling square against the top door track. The 40-foot cars have five welded panels on each side of the door. Measurements were taken from a 40-foot PS-1 kit. The fifth weld line separating the fifth and sixth panels is heavier where the new center section is added.



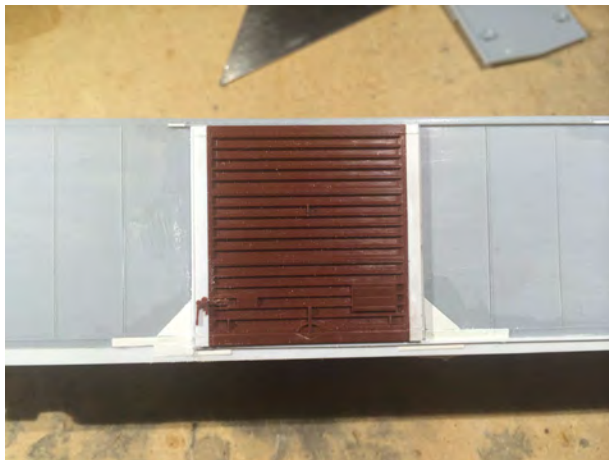
*New weld lines are scribed using a modeling square; measurements were obtained from a 40-foot PS-1 kit.*

## *New York Central's Fifty-Foot PS-1 Boxcars*

### *Part 2: Lot 136-B NYC #s 215000 - 215399*

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Other modifications to the body included extending the top door track and a new lower door track from a piece of .015x.020 styrene strip on edge. A new left side doorstop was made from a piece of .020x.020 strip. Then door braces were made from .005 sheet styrene. A .040x.060 block for the stirrup step was glued to the lower left side of the body to mount the stirrup. Stirrup steps on the left side of the body came from leftover parts that are included in Branchline AAR boxcars kits, but Titchy Train Group 3047 is an acceptable alternative. The kit's left side grab irons and shortened right side ladders were then installed.



*The completed sides showing with new lower door track and reinforcing brackets around the bottom of the door.*

As innovative as NYC was in some respects, they also held onto some old traditions long after abandoned by other railroads. The use of polling pockets had largely disappeared from the boxcar built after the end of World War II. However, the New York Central continued the use of polling pockets on boxcars built up to 1960. While the lot 136-B was rebuilt in 1967, the lot 842-B boxcars were rebuilt from were those built in 1952. The polling

pockets were retained during the rebuild. No HO-scale models of the PS-1 have polling pockets. To replicate these I took a small piece of .080x.080 styrene and built up the corners. After the glue had dried, I rounded the corners to match the corners of the ends. The pockets on the 136-B are a crescent shaped piece of steel welded to the corner instead of a stamped cup like other boxcars. To make the crescents, I took a 3/32" styrene tube and reamed the inside with a #50 drill. This took the wall thickness of the tube to about .010. Placing the tube on the shank of the drill, I sliced thin rings. These were cut in half then they were rolled on the shank of a smaller drill to bring the crescents to .080 to fit on the built up corners.



*A small crescent of styrene was formed to represent the polling pocket.*

Kadee scale whisker couplers in the kit's boxes were added along with DA coupler cut levers. The model's weight is light compared to the NMRA 20.1 standard. Twenty pennies were glued to the inside to bring the model to the standard weight. Then it was painted with TruColor Paint NYC green, while the roof was painted silver, and the frame black. The car was lettered with Microscale NYC green boxcar decals. Light washes of gray and



# New York Central's Fifty-Foot PS-1 Boxcars

## Part 2: Lot 136-B NYC #s 215000 - 215399

brown diluted acrylic craft paint fill out the weathering. I didn't want a heavy weathering applied to the model; however, photos of stretched boxcars show weathering patterns. The new center three roof panels are far less weathered than the original panels. So to replicate this, the center panels were not weathered while all the other panels were weathered.



*Seth's Completed model. Notice how the three center roof panels are not weathered suggesting the boxcar has had a section added to the center.*



*Another view of the completed boxcar.*

Materials List	
<b>Archer</b> 88026 rivet decals	263 .100 styrene channel 272 .080 styrene I beam 9009 .005 sheet styrene
<b>Branchline Trains</b> AAR boxcars stirrups	<b>Kadee</b> 158 scale couplers 1562 ASF A-3 HGC Trucks with .088 wheels 2032 Miner brake wheel No Part Number 9" Youngstown doors
<b>Detail Associates</b> 2202 Eyebolts 2506 .019 brass wire 6215 Coupler Lift bars	<b>Microscale</b> 87-58 NYC 40 and 50-foot green boxcar decals
<b>Evergreen Scale Models</b> 100 .010x.020 styrene strip 102 .010x.040 styrene strip 104 .010x.080 styrene strip 105 .010x.100 styrene strip 110 .015x.020 styrene strip 120 .020x.020 styrene strip 121 .020x.030 styrene strip 123 .020x.060 styrene strip 124 .020x.080 styrene strip 143 .040x.060 styrene strip 144 .040x.080 styrene strip 145 .040x.100 styrene strip 164 .080x.080 styrene strip 219 .025 styrene rod 223 3/32" styrene tube	<b>Moloco</b> 305 Miner handbrake housing
	<b>TruColor Paint</b> 43 NYC green
	<b>Miscellaneous</b> 20 pennies Silver paint



## SPR Backshops Custom Build of a WrightTrak Resin Caboose Kit



[http://www.nycshs.net/Wright-Trak\\_c\\_85.html](http://www.nycshs.net/Wright-Trak_c_85.html)

# Building an American Model Builders Big Four Freight House Kit

Article by Noel Widdifield

Photos by Noel

I have always been a Big Four fan, having grown up near Anderson, IN. So when the Society offered the American Model Builders NYC Big Four Freight House, I decided to give it a try. So I purchased one from the NYCSHS Collinwood Shop.

the instructions before beginning to build the kit. I did that, and it made a big difference in the construction.

Before I began the assembly, I spray painted all of the parts with a gray primer.



The kit came in a very nice large box with a detailed set of drawings and instructions and all of the necessary parts.

I have used Krylon spray cans for most of my modeling projects in Large Scale, so I decided to continue that practice in HO-scale. I used the blue masking tape to hold the light wood parts while spraying.



After allowing the parts to dry thoroughly overnight, I painted the parts with the two NYCS building paints of dark and light green.

The instructions were printed clearly and were easy to read and understand. I followed them completely.

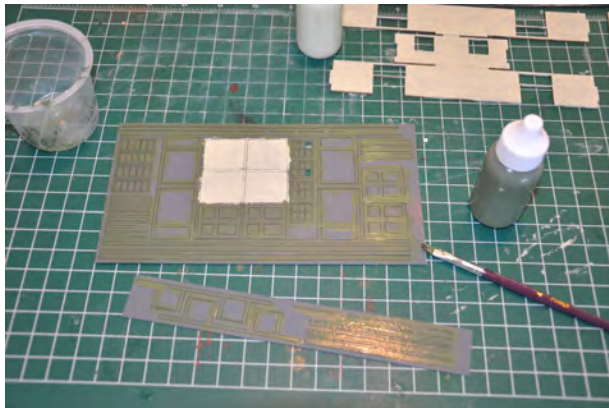
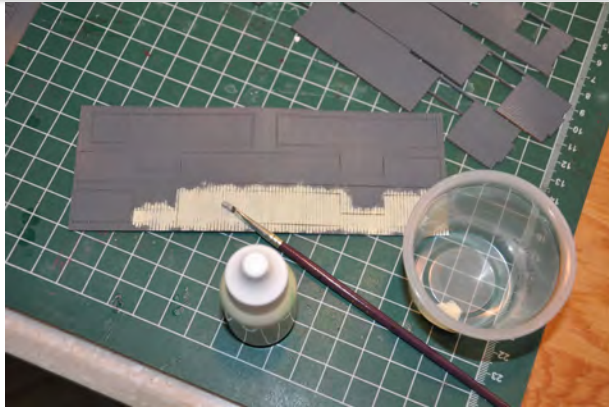


The instructions said to review all of the parts supplied and read and understand

The parts ready for painting in the light and dark green paint used on the Big 4 buildings.



## *Building an American Model Builders Big Four Freight House Kit*

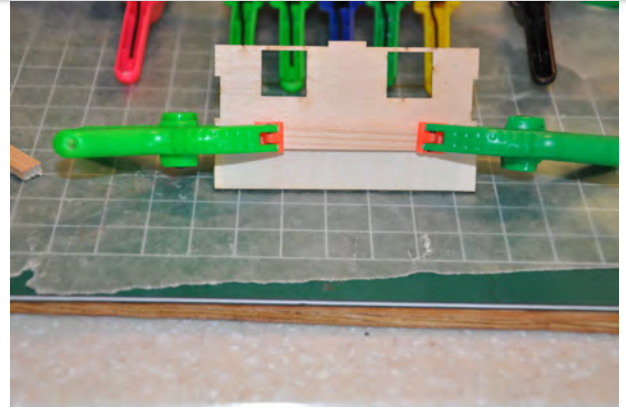


*The NYC SHS paints were brushed on the parts for the building. They covered in a single coat.*

The two parts of the building's sides and ends were glued together with Titebond wood glue and clamped until dry.



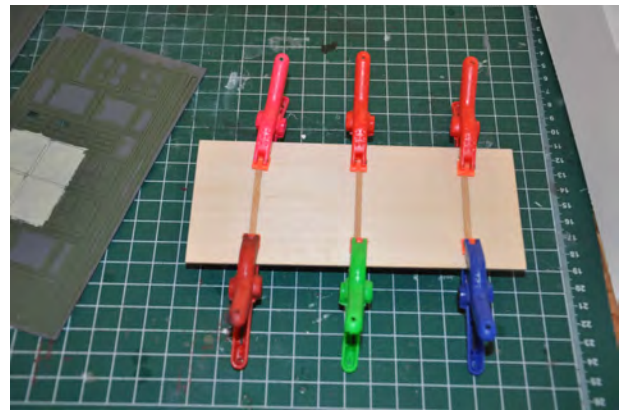
*I used a large number of small clamps to secure the two parts of the long sides. All of the gluing was done on wax paper to prevent the glue from attaching the parts to the cutting board.*



*Small pieces of scrap basswood were glued and clamped in place to reinforce the sides after the glue had dried.*

Once the sides and ends were glued and reinforced, I painted the inside of the building using the Krylon primer gray.

I then turned to the building floor piece and added reinforcing strips of basswood on the bottom to prevent warping.

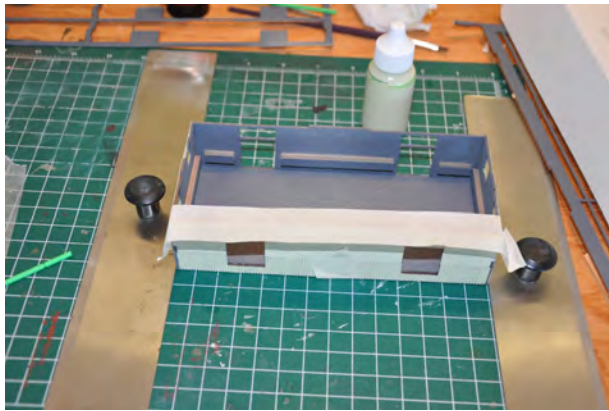


*I used the small c-clamps to hold the reinforcing strips of basswood in place while the glue dried.*

Following the instructions, I next glued the four small floor supports in place on the sides and ends of the building. I then glued the four sides of the building and the floor together making sure that all of the parts were level and square. The floor supports allowed me to position the floor in place against the one building side and

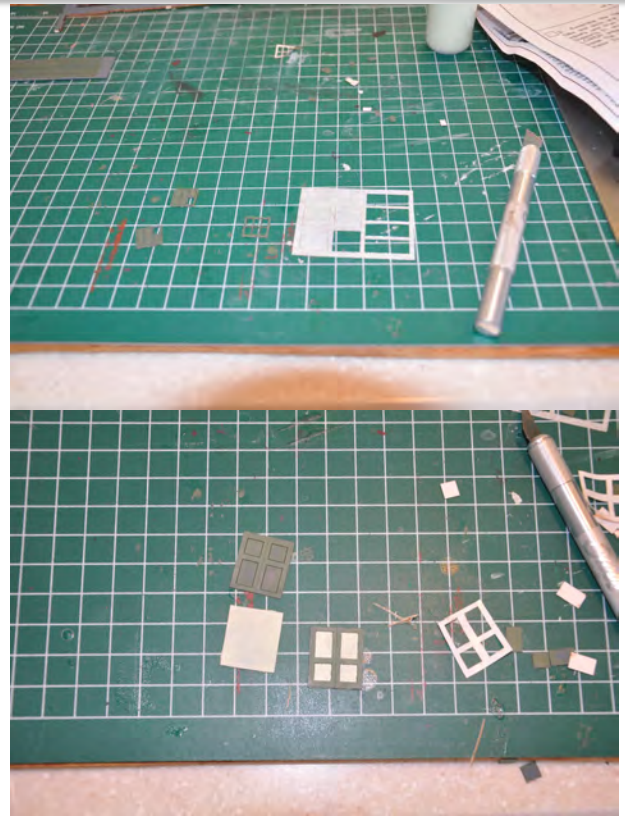


the two ends. I held these pieces in place for a couple of minutes until the glue began to set up and make the three sides of the structure fairly secure. I then fitted the final side into place using the floor supports to hold it. Again I held the side in place for a few minutes until the glue set up enough to allow me to clamp it using masking tape. I found that my large clamps were not large enough to use here. Looks like I need to buy some larger clamps, although the tape worked well.



*Masking tape and two cutting guides were used to hold the building together while the glue dried.*

Assembling the windows and doors required patience, as the parts are very small. I used a hobby knife with a new, sharp blade to remove these from the wood sheet. I pre-painted all of these with the correct colors before I removed them from the wood sheet. All of these parts have a peel and stick backing on one side. There were eight windows and four service doors for the building. The doors had a back piece that was scribed and a front piece that glued to the back piece. The window and doorframes and trim pieces are removed the same way and they are very tiny



*The windows and doors must be removed from the wood sheet and care must be taken to ensure that the pieces are not damaged.*

The trim pieces and window and door frames are pressed into place taking care to be sure they are positioned correctly. All of these parts are peel and stick so there is no worry about excess glue.



*The trim pieces and window and doorframes are very small but the peel and stick backing makes the job easy.*

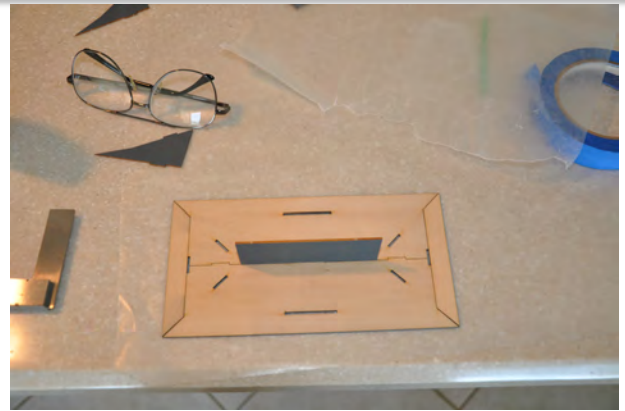
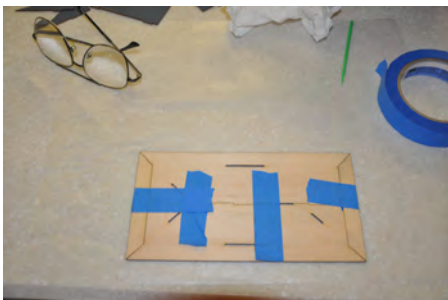
I also placed the peel and stick trim pieces on the front, rear and end walls and then fastened the two corner trims to each corner of the building. I positioned all of the doors in the closed position with the exception of one on the front wall. I thought that leaving them partially open would allow some minor interior detailing later.



*The building kit with the windows and doors installed.*

The directions explained that the next part of the project would be the most tedious. This was the construction of the roof. There were nine pieces to the sub-roof and eight pieces for the roof.

I assembled the sub-roof following the directions and that involved gluing the four scribed panels together on a flat surface and then attaching the roof former into the slots in that assembly making sure that it was glued perfectly vertical.



*The sub-roof went together very easily, but care had to be taken to be sure that the roof former was perfectly vertical.*

After the glue had dried completely for this assembly, I added the four roof corner formers by gluing them in the four slots at each end of the vertical roof former.

Once the glue had dried, I spent a considerable amount of time test-fitting the roof panels to the sub-roof frame. All of these eight panels fit together using tabs and slots and all must be positioned exactly right to make the roof look correct. Once I felt comfortable about the final placements of the panels, I glued the top four panels in place on the sub-roof frame. I then attached the final four lower panels in place.



*Three of the four top roof panels have been glued in place with the fourth one to be glued.*



## *Building an American Model Builders Big Four Freight House Kit*

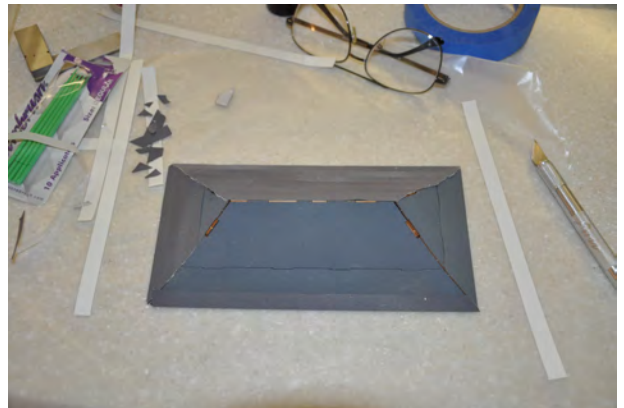
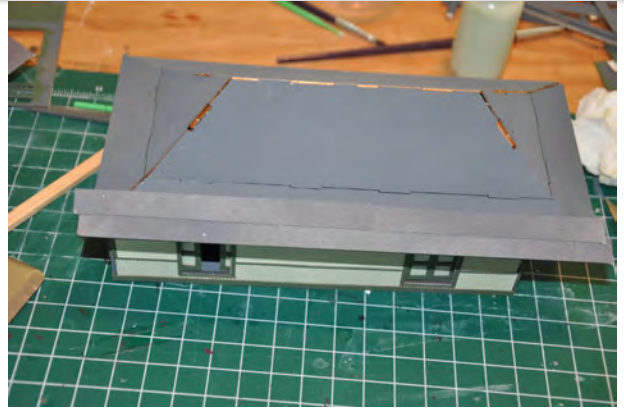


*The first of the lower roof panels is in place.*



*All four roof panels have now been glued together forming the distinctive roofline of the Big Four stations.*

The next project after the glue for the roof assembly was fully dried, was to attach the rolled roofing strips to the roof. I began at the bottom each part of the roof and worked my way up to the top on each of the four sides of the roof. I made sure that I made some vertical breaks in the roofing strips. I had already spray painted the roofing strips with flat black paint and then lightly added some rust red and white paints for just a dusting.



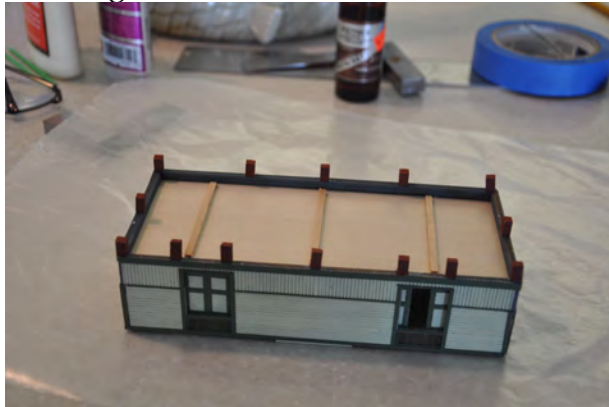
*The rolled roofing strips are glued in place starting at the bottom of the roof and working up to the top. Once all of them are in place, folded panels are used to replicate the flashing strips along the rooflines.*

The kit comes with 18 cast white metal brick piers to be placed under the building. I spray painted them with red primer and then used a white wash to leave "mortar" on the piers. I then glued them in place on the four corners of the



## *Building an American Model Builders Big Four Freight House Kit*

building and spaced the remaining piers along the front, back, and sides of the building.



*Here you can see the brick piers glued into position along the front, back, and sides of the building.*

I attached the roof to the building and assembled the loading dock. This required building a small jig to allow correct spacing of the joists on the long pier supports. I glued a joist to each end of the long pier supports and placed that assembly in the three gluing jigs that positioned the pier supports in the correct position.



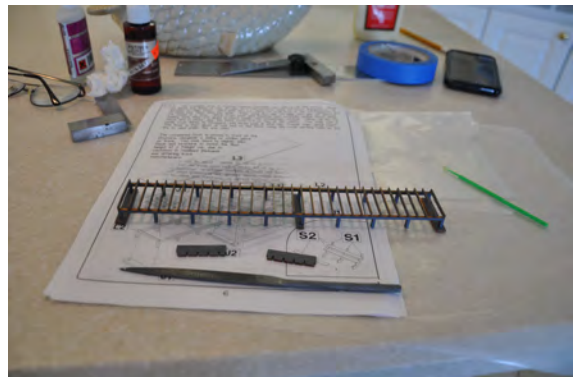
*I used a roll of masking tape and the glue bottle to hold the loading dock pier supports in place while the glue dried.*

Using the joist jib I glued the joists to the long pier supports starting at each end and working towards the middle until they were all in the correct position. This

sound difficult but once you begin it is easy to do.



*The jig supplied with the kit made it easy to position the joists properly. It was much easier than I thought it would be.*



*The completed loading dock pier is now ready for the deck to be glued in place.*

Once the glue had dried, I glued the dock floor in place and held it in place with one of my metal cutting guides until the glue was dry.



*The deck was glued in place after the pier was finished.*

## *Building an American Model Builders Big Four Freight House Kit*

The final step was to assemble the stair unit. It has three stringers and three treads and the treads have peel and stick on the back. This looked and sounded easy but turned out to be very difficult. It required three hands, and I only had two, but eventually it went together and I then glued it to the end of the loading dock.



*This final step of putting the steps together was the most difficult part of the whole project.*

The project took about eight hours over a period of a week to complete, but it is my first HO-scale building for my new HO-scale layout I am beginning to build in our winter home.

It has been torture for me to watch the HO-scale models being sold in the Collinwood Shop. There have been so many beautiful locomotives and rolling stock for sale over the last couple of years, and I could just no longer resist the urge to buy some of them. So now I have joined the mainstream of model trains again. Back to HO-scale modeling, even though on a small scale.



*This is the Big Four freight house kit completed and ready to go on my layout.*

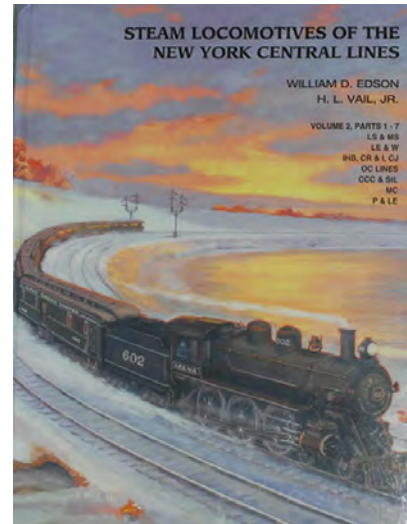


# NYCSHS Steam Locomotive Books

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You simply must have...*Steam Locomotives of the New York Central Lines, Volumes 1 & 2* by William D. Edson and H. L. Vail, Jr. assisted by Edward L. May. The ultimate authority on steam and electric locomotives for the *DeWitt Clinton* of 1831 to P&LE 9406 of 1948. These books represent a lifetime of research for each of the compilers and are a fitting memorial, not only to the legacy of the New York Central Lines but also to the authors' untiring efforts to preserve that legacy for all of us. These are a very limited edition and only a few remain. They will not be printed again. Don't miss this rare opportunity to save on this valuable resource.

**Order from our store: ([www.NYCSHS.net](http://www.NYCSHS.net)) with PayPal, credit card, check, or money order. Or you can send a check to: NYCSHS, P. O. Box 130, Gates Mills, OH 44040-0130.**

*Volume 1: New York Central & Hudson River, Boston & Albany.* This 310-page, hard cover volume includes: 320 photos, 184 drawings and maps. Only \$30 plus postage. Ohio residents include \$2.40 state sales tax for a single volume. (\*Non-US postage charged at actual cost.)

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OR GET BOTH VOLUMES FOR ONLY \$49.00!

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## Photo Section The Observation Car



### Larry Faulkner's SS 37 Tower



*Larry is just about finished doing some heavy modifications to Russ's new NYC/B&A wooden tower kit to more closely resemble SS 37 in Peekskill. He will do one more of these wooden tower kits to model Tower 39 that guarded the drawbridge over Annesville Creek on Peekskill Bay.*

*The West Shore had wooden towers that were built to the same NYC Standard plan. Here is a photo of his SS 37 Tower project. Just some minor repairs and weathering are all that's left to do on the tower itself. The rods and hangers might take some time, but Manuel Duran-Duran and I are making the hangers and rollers with a laser machine and using brass rod for the rods. Watch for his article in the next NYCentral Modeler.*



*This is a photo of the real tower that Larry is modeling. He and Manuel continue to produce excellent models of the real thing using their modeling skills and Manuel's skills in creating scale drawings from photos. They make a great NYCSHS team.*



## Photo Section The Observation Car



### Larry Faulkner's SS 37 Tower (Continued)



*Larry is working to complete his model and has done a very nice job of modeling the switch levers and floor in the tower. The levers are a new kit from GCLaser, kit #1293. They're not really wood, mostly a heavy cardstock, but they look nice. (Left Photo)*

*He is making the hangers from scratch using Evergreen strip and his Dremel drill press with a scale ruler clamped for a fence and guide. Manuel Duran-Duran plans to cast them or have some 3D printed then cast.*

*He is using Bill Darnaby's Jan. 2004 MR article as a rough guide. The actual rods are Detail Associates .015" brass rod that he will blacken, paint, and weather. He plans on building a small module for the SS 37 interlocking.*





## Photo Section The Observation Car



### Dave Mackay Continues to Focus His Efforts on NYC Towers



*The tower SS DU at Dumont, NY on the River Division that began Dave's quest for finding a manufacturer for a kit for these towers. In the next NYCentral Modeler he tells us how the final version of this kit came about. It has been a long journey for Dave, but the result is the excellent Kit from The N Scale Architect exclusive NYCSHS kit now for sale in the Collinwood Shop.*



*The result of all of Dave's hard work and attention to detail. He tells us how it happened in the next edition. Be sure to watch for that coming in April. These kits are now available in the Collinwood Shop in HO- and N-scale.*

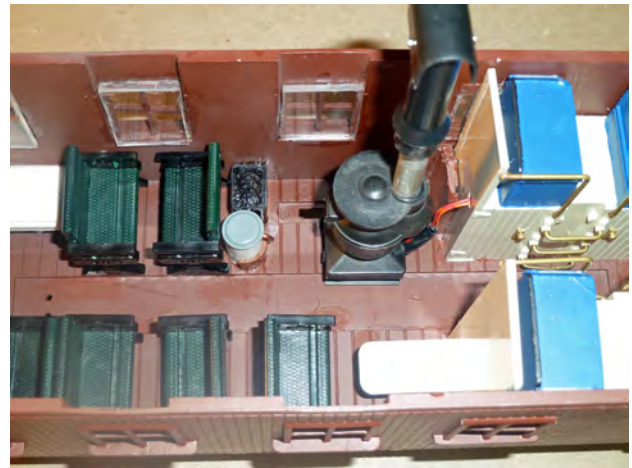
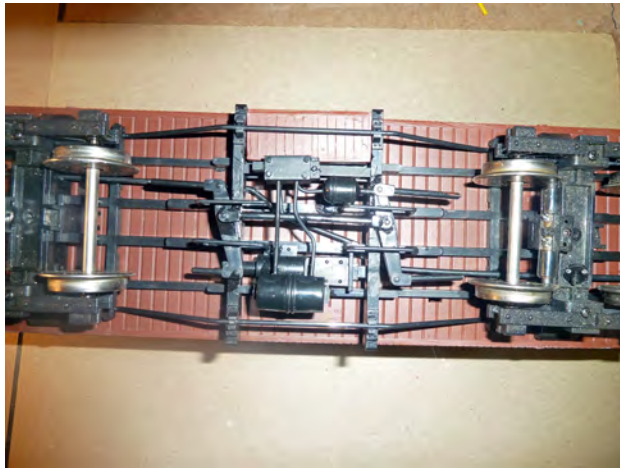




## Photo Section The Observation Car



### Will Boyle Kitbashed a G-Scale MC/ NYC St. Thomas Caboose



*Will took a generic 1/24-scale model and kit bashed it into a caboose depicting one for his railroad. Hopefully it will inspire some of you to share some of your work with us. Our readers love photos and articles showing how to do these projects. It only takes a short time to document your projects with photos as you go and then only a little more time to write up an article to share with us. We even have some materials to help you do it. Why not give it a try for us?*

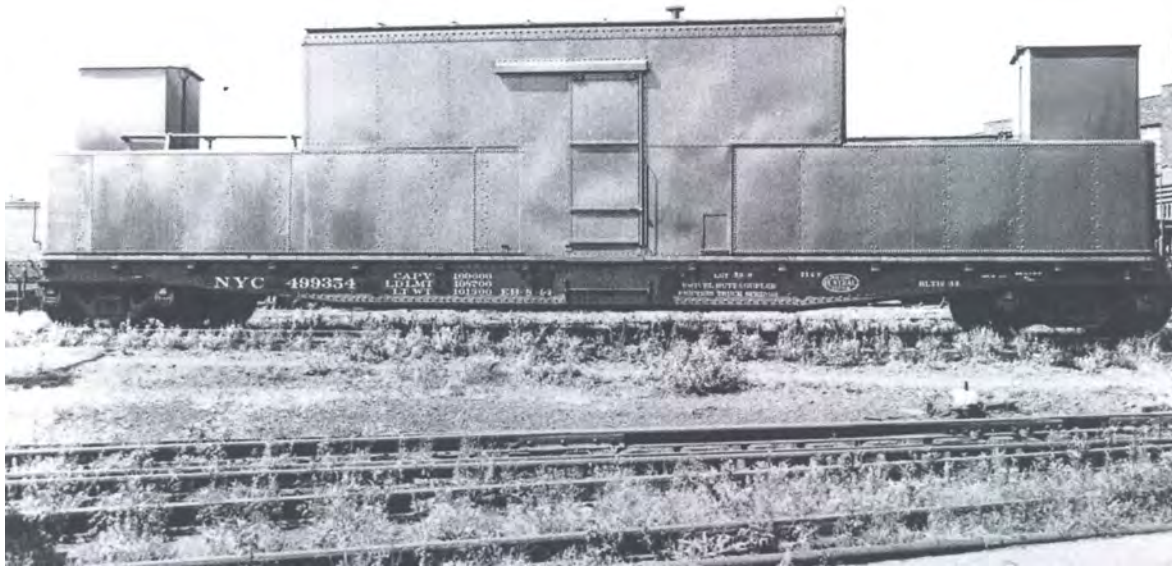




## Photo Section The Observation Car



### NYCentral Modeler - "Mystery Photo"



Tell us what piece of equipment is in this drawing. Send your answers to [NYCSHS@verizon.net](mailto:NYCSHS@verizon.net)

### Dave Staplin Shares his Knowledge and Modeling Skills for NYCS Passenger Cars



*This photo is the wall display in the Dave's basement where from top to bottom are: the Advanced Empire State Express, the Empire State Express, the Twentieth Century circa 1940, the Commodore Vanderbilt, first section of 1948 Century, second section of 1948 Century, New England States, Empire builder, California Zephyr, empty row, Denver Zephyr.*

*Dave has quite a number of passenger cars and we are looking forward to his article on them in the next NYCentral Modeler.*







## Photo Section The Observation Car



### Final Thoughts By Noel Widdifield

I will continue this section to show what is now available in the Collinwood Shop. [Click here to go to the store.](#) You can see that we have added a large number of new models since the last issue. There are also still a few of your favorites left from those we listed before. Watch for upcoming new models.

## N-Scale

### *Bluford Shops 36' USRA 2-Bay Hoppers*

#### *NYC & CCC&StL*



### *Bluford Shops 3-Bay B&A Offset Side Hopper*

#### *B&A & P&LE*



[Click Here to Order](#)



## Photo Section The Observation Car



### *ESM NYC GSC Well Car (Second Run)*



[Click Here to Order](#)

### *Rapido NYC 73'6" Baggage/Express Car*



[Click Here to Order](#)



Photo Section  
The Observation Car



*The N Scale Architect B&A SS 65 Tower*



**HO- & N- Scale**  
**[Click Here to Order](#)**

*The N Scale Architect NYC Lines East Brick Tower*



**HO- & N- Scale**  
**[Click Here to Order](#)**



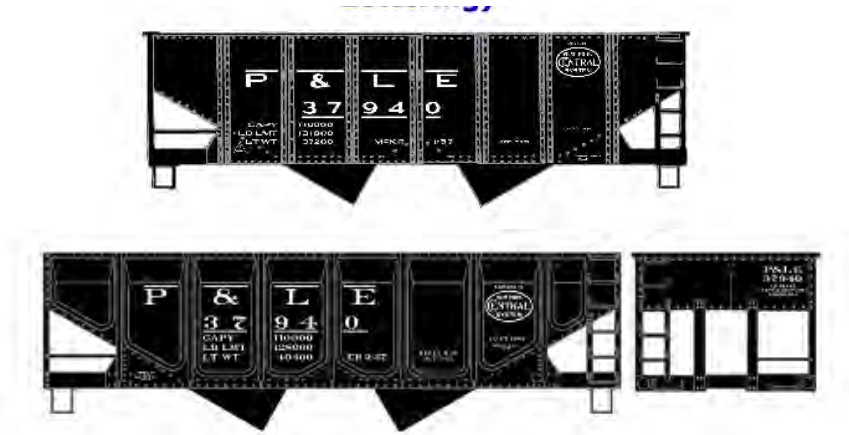


# Photo Section The Observation Car



## HO-Scale

### *Accurail P&LE (NYC) 55-Ton Panel Side Hoppers*



Gothic or Roman Lettering

[Click Here to Order](#)

### *Rapido NYC ALCo FA-2s*



### *Rapido NYC RDCs*



[Click Here to Order](#)



## Photo Section The Observation Car



# NYC DFA/B FM C-Liners Are Here



TLT 500153	NYC C-LINER A/B SET, DC #5015 #6901 - CIGAR BAND	TLT 500153S
TLT 500154	NYC C-LINER A UNIT, DC #5017 - CIGAR BAND	TLT 500154S
TLT 500155	NYC C-LINER 4 AXLE A/B SET #5007 / 5104, DC - LIGHTNING	TLT 500155S
TLT 500156	NYC 4 AXLE C-LINER #5011, DC - LIGHTNING	TLT 500156S

We have just been notified by True Line Trains that they will be delivering the NYC DFA/B FM C-Liners to us in mid-Jan 2106. These models were first announced in early 2013 and we allowed pre-orders at that time. We expect these to be delivered to us in Jan and we have a few extra now for sale in the Collinwood Shop.

In order to purchase these beautiful models you may go to the NYCSHS Collinwood Shop online store, sign in, place your order and pay for them. [http://www.nycshs.net/NYC-Fairbanks-Morse-C-Liners\\_c\\_28.html](http://www.nycshs.net/NYC-Fairbanks-Morse-C-Liners_c_28.html)

If you wish to pay for them without going to the store, please send us an email at [NYCSHS@verizon.net](mailto:NYCSHS@verizon.net) and we will provide you with the price including shipping and Ohio sales tax (If you are a Ohio resident) and the address to mail in your order.

The prices for them have not changed and they appear to be really nice models of this NYC locomotive.

They are available in DC or DCC with sound. You may order them as singles or in pairs and they come in Lightning Stripes or Cigar Band livery. NYCSHS members enjoy a 20% discount off MSRP. Shipping is extra and Ohio residents must pay 8% Ohio sales tax.

This has been a long wait, but the end result has been worth the wait.

**[Click here to order!!!](#)**

**We have the opportunity to order the NYC Lines East Brick Tower in O-Scale but we want to see if there is any desire from our members to purchase these kits. We currently offer them in HO- & N-scale and they are selling very well.**

**If we offered it in O-scale it would measure about 16" length X 4" width and 7" high. It would have an MSRP of \$262.50 and a NYCSHS members' price of \$210.00. We are not sure we could sell enough at this price to justify the minimum order we would have to place with the manufacturer.**

**If you would be interested in purchasing one of these kits, please send us an email at [NYCSHS@verizon.net](mailto:NYCSHS@verizon.net) and express your interest.**



## Photo Section The Observation Car



### NYCSHS Exclusive Models



### *HO-Scale NYC 19000 Caboose*

*(TrueLine Trains) Pre-order only. Email [NYCSHS@verizon.net](mailto:NYCSHS@verizon.net)*

True Line Trains just informed us that these are the next model they will produce. They now expect to make them available sometime in 2016. You can always see the latest status of these models and all of the models we are offering by going to our website and looking under the "Modeling Resources" tab and the pull-down menu there to "Model Shipping Schedule". <http://nycshs.org/model-shipping-schedule/>

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### *TrainMaster LLC HO-Scale NYC Section House Kit*



This section house was the mainstay of track maintenance for many decades on railroads. The NYC placed their unique structures every few miles on the right of way so crews had ready access to tools and supplies. Also, a speeder was stored there so the track gang could move easily up and down their assigned stretch of track. This is an exclusive NYCSHS model.

*[Click Here to Order](#)*





## Photo Section The Observation Car



### **Some More Thoughts About NYCSHS Model Offerings**

As you can see on the last few pages and in our Collinwood Shop, <http://www.nycshs.net/>, we offer a very large selection of NYCS models at excellent discounts to members. We also regularly offer unique models that cannot be obtained elsewhere. We are dedicated to providing the most authentic NYCS models available and to providing them in a timely manner after orders are placed, but we have no control over the final production of any of the models offered. We do apologize for the inconvenience and frustration these delays may have caused and hope you will continue to support the Society.

The NYCSHS Modelers Committee works hard to monitor all of the manufacturers who produce NYCS models so that we can provide you with the latest information on those models. You will find much of that covered in the "What's New for NYC Modeling" section of this magazine.

We also monitor very closely those manufactures that we have "store" agreements with for their new NYCS products so that we have the opportunity to create unique road numbers to offer to our members. All of these are offered at the NYCSHS members' discount.

We also periodically send out surveys to understand what models you would like to have from us. We try to pay attention to your responses and give you what you ask for. We have even made good strides in offering models in N-, O-, and S-scale although we sell very few of these for the amount of effort they require to offer.

We pledge to do our best to deliver the orders to you as soon as we receive them from the manufacturer. Please continue to support the Society by ordering the NYCS models, and we ask for you to be patient with us as we try to provide them to you as quickly as they are produced. Remember that when you purchase a model from the Society, you save money and you contribute to the running of the Society. The purchases from the store supplement the dues and keep the Society from having to raise the dues to support our continued operation. You can help us by sending us emails telling us how we are doing and what else you would like us to do. [NYCSHS@verizon.net](mailto:NYCSHS@verizon.net)

#### **The NYCSHS Modelers Committee**

The Collinwood Shop has been a tremendous success for the Society. We have been able to offer many different NYCS-related things for sale to our members. There is a wide selection of clothing, books, DVDs, drawings, calendars, memorabilia, and models available. The sales from all of these offerings have allowed the Society to avoid raising the membership dues. The costs continue to grow for all of the things we are providing to members. You are urged to take the time to review all of the things offered for sale. Remember that every dollar you spend in the store goes directly to pay for the benefits you enjoy.

Do you love the NYC? How about helping the NYCSHS preserve the memory. Do your part.



If you have read this edition of the *NYCentral Modeler*, you can't help but be impressed by the fine modelers out there in our membership. You also should have been impressed with all of the things we are doing to help you model the NYCS. **We have been publishing great articles from many of you in all scales. We note that several other Historical Societies' modeling magazines have gone out of production from lack of member articles. Don't let that happen to us.**

We really do need your help to keep all of the momentum going in the Society. A lot of work is being done to improve our support to members. We need articles and photos for this magazine. We need some people to work with us on the Membership Committee. We could use more help in the NYCSHS archives, backup people for the website and the Collinwood Shop, and someone with some financial knowledge to help out our Treasurer.

We all are busy, but it only takes a few hours a month to help us out. We have gotten a few new volunteers now working with us. They believe that the Society is a priority for them. **How about you? Why not join the fun and excitement of involvement in something that is really worthwhile. Just do it!!!** [NYCSHS@verizon.net](mailto:NYCSHS@verizon.net)

Watch the website, [www.nycshs.org](http://www.nycshs.org), for more information and updates. For questions and inquiries, contact Noel Widdifield at [NYCBigFour@verizon.net](mailto:NYCBigFour@verizon.net).



As we head into the winter modeling season, the NYCHS would very much like to include an article by you in the next issue that will be released in the 2<sup>nd</sup> Qtr. 2016 edition. You should be doing a lot more with your layout or modeling as we return to winter. If you will be building things for your railroad or running your trains, this would be a good time to take some photos and do an article for us. We really would love to hear about your modeling or see your layout. We offer a good opportunity to showcase your talents or collection. Even if you don't feel you can write an article, just send us some photos of your layout. We need them for all of our publications, but to make the next one, send them to us by February 15, 2016. [NYCSHS@verizon.net](mailto:NYCSHS@verizon.net)

## Preview Of 2nd Quarter 2016 issue



Phil Darkins hopefully returns to tell us about learning to use an airbrush to paint his first resin kit.

Look for a Peekskill SS 37 tower project by Larry Faulkner in the 2<sup>nd</sup> Qtr. 2016 edition.



Two of our modeling committee, Dave Mackay & Dave Staplin, have promised articles in the next edition.

We are anticipating Bob Shaw's next article as he continues to build his NYC empire in O-scale.

